

LAND USE APPLICATION SUMMARY

Property Location: 501, 507, 515 and 523 South 8th Street, 502 and 518 South 9th Street and 811 5th Avenue South

Project Name: Kraus Anderson Block Redevelopment

Prepared By: Hilary Dvorak, Principal Planner, (612) 673-2639

Applicant: Kraus-Anderson

Project Contact: Michael Korsh

Request: To construct a planned unit development including a 306-unit residential building, a 161-room hotel with a restaurant, a 12,000 square-foot brewery, a 13,000 square-foot event center, a 103,000 square-foot office building for Kraus-Anderson and 520 underground parking spaces.

Required Applications:

Conditional Use Permit for a Planned Unit Development	To allow for a Planned Unit Development.
Site Plan Review	For a planned unit development including a 306-unit residential building, a 161-room hotel with a restaurant, a 12,000 square-foot brewery, a 13,000 square-foot event center, a 103,000 square-foot office building for Kraus-Anderson and 520 underground parking spaces.
Registered Land Survey	RLS-83

SITE DATA

Existing Zoning	B4N Downtown Neighborhood District DP Downtown Parking Overlay District
Lot Area	109,571 square feet / 2.52 acres
Ward(s)	7
Neighborhood(s)	Elliot Park, adjacent to Downtown West
Designated Future Land Use	Mixed Use
Land Use Features	Growth Center (Downtown)
Small Area Plan(s)	<u>Elliot Park Neighborhood Master Plan</u> (2003) <u>Downtown East/North Loop Master Plan</u> (2003)

Date Application Deemed Complete	February 17, 2016	Date Extension Letter Sent	Not applicable
End of 60-Day Decision Period	April 17, 2016	End of 120-Day Decision Period	Not applicable

BACKGROUND

SITE DESCRIPTION AND PRESENT USE. The property occupies the entire block bounded by South 8th Street, Portland Avenue, South 9th Street and 5th Avenue South. The property is currently occupied by the 34,000 square-foot Kraus-Anderson office building and a 299 space surface parking lot.

SURROUNDING PROPERTIES AND NEIGHBORHOOD. The site is surrounded by a mixture of residential developments of varying densities, hotels, office buildings, commercial establishments and both surface and structured parking facilities. The site is located in the Elliot Park neighborhood.

PROJECT DESCRIPTION. The applicant is proposing to construct a planned unit development involving three separate buildings. The development includes a 306-unit residential building, a 161-room hotel with a restaurant, a 12,000 square-foot brewery (Limited production and processing), a 13,000 square-foot event center, a 103,000 square-foot office building for Kraus-Anderson and 520 underground parking spaces.

The residential building would be constructed along Portland Avenue. The building would be 17-stories or 185 feet in height. The principal entrance to the building would be along Portland Avenue and the ground floor units would also have individual entrances from the street. The building would have a total of 306 dwellings with a mixture of studios, one-, two and three-bedroom units. There would be outdoor amenity space on the ninth floor of the building.

The proposed 'Brewtel' (boutique hotel/ brewery/innovation center) would be constructed on the corner of South 9th Street and 5th Avenue South. The hotel portion of the building would be eight-stories or 103 feet in height and the brewery portion of the building would be five-stories or 70 feet in height. The two portions of the building would be connected by a two-story glass atrium. The principal entrance to the building would be along 5th Avenue South. The hotel would have a total of 161 rooms and there would be a restaurant and bar on the ground level. The brewery would be home to Finnegans, a non-profit brewery that donates 100 percent of its profits back to the community. An event center and innovation space would be located on the upper levels of the brewery portion of the building.

The proposed office building would be located on the corner of 5th Avenue South and South 8th Street. The building would be five-stories or 85 feet in height. The principal entrance to the building would be located along 5th Avenue South. There would be outdoor amenity space on the fifth floor of the building.

For the development there would be 520 underground parking spaces provided. Access to the parking garage would be located along South 8th Street. An outdoor multi-purpose courtyard is proposed in the middle of the development. The courtyard would be used by the proposed development for a variety of activities including outdoor dining. Access to the courtyard is provided from the two-story glass atrium or from a series of walkways that lead into the middle of the block from the surrounding public sidewalks. Also on street level there would be a one-way drive that leads from 5th Avenue South to South 9th Street. The drive would be used for pedestrian drop-off for the different uses on the block as well as for loading purposes.

RELATED APPROVALS. The City of Minneapolis prepared a Mandatory Environmental Assessment Worksheet (EAW) for the Kraus-Anderson Block Redevelopment project according to the Environmental Review Rules of the Minnesota Environmental Quality Board (EQB) under Rule 4410.4300 Subp.19, Residential development D. 375 attached units in a city within the seven-county Twin Cities metropolitan area that has adopted a comprehensive plan under Minnesota Statutes, section

473.859; and Subp. 32. Mixed residential and industrial-commercial projects with a sum of quotients exceeding 1.0. On February 12, 2016, the Minneapolis City Council approved the staff recommendation that the EAW was adequate and that the preparation of an Environmental Impact Statement (EIS) was not required.

PUBLIC COMMENTS. A letter of support from the Elliot Park Neighborhood is included with the report. Any additional correspondence received prior to the public meeting will be forwarded on to the Planning Commission for consideration.

ANALYSIS

CONDITIONAL USE PERMIT - PLANNED UNIT DEVELOPMENT

The Department of Community Planning and Economic Development has analyzed the application to allow a Planned Unit Development based on the following findings:

1. *The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare.*

The establishment of a planned unit development that includes 306 dwelling units, a 161-room hotel with a restaurant, a 12,000 square-foot brewery, a 13,000 square-foot event center, a 103,000 square-foot office building and 520 underground parking spaces would not be detrimental to or endanger the public health, safety, comfort or general welfare. Redevelopment of the block would be beneficial to the neighborhood. The development would provide activity on the site at all hours of the day.

2. *The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.*

The establishment of a planned unit development that includes 306 dwelling units, a 161-room hotel with a restaurant, a 12,000 square-foot brewery, a 13,000 square-foot event center, a 103,000 square-foot office building and 520 underground parking spaces would not be injurious to the use and enjoyment of other property in the vicinity or impede the normal or orderly development and improvement of surrounding property. The site is surrounded by a mixture of residential developments of varying densities, hotels, office buildings, commercial establishments and both surface and structured parking facilities. The proposed development would complement the existing uses in the area.

3. *Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.*

The Public Works Department has reviewed the preliminary plans and will review the final plans for compliance with standards related to access and circulation, drainage, and sewer/water connections. The applicant will be required to continue to work closely with the Public Works Department, the Plan Review Section of CPED and the various utility companies during the duration of the development to ensure that all procedures are followed and that the development complies with all city and other applicable requirements. The applicant is aware that the final plans are expected to incorporate any applicable comments or modifications as required by the Public Works Department.

4. *Adequate measures have been or will be taken to minimize traffic congestion in the public streets.*

There is no minimum parking requirement for any use in the B4N zoning district. However, in the downtown zoning districts, when residential parking is provided, one guest parking space is required to be provided for every 50 dwelling units within the building. There will be parking provided for the residential portion of the development. There will be 306 dwelling units constructed as part of the development which requires a total of six guest parking spaces. There will be a total of 520 parking spaces provided in two levels of underground parking for all of the uses within the planned unit development.

A Travel Demand Management Plan (TDMP) was completed for the proposed development. The study concludes that, overall, the proposed uses have minimal traffic impacts on intersection operations, all intersections have adequate capacity to accommodate the number of trips that will be generated and, therefore, impacts on existing roadway operations are minimal. In addition, the site has been designed so all truck maneuvering and loading will occur on site.

5. *The conditional use is consistent with the applicable policies of the comprehensive plan.*

The proposed development would be consistent with the following general land use policies of *The Minneapolis Plan for Sustainable Growth*:

Land Use Policy 1.1: Establish land use regulations to achieve the highest possible development standards, enhance the environment, protect public health, support a vital mix of land uses, and promote flexible approaches to carry out the comprehensive plan.

- I.1.3 Encourage the use of flexible regulatory options that promote high quality development, such as the Planned Unit Development (PUD) tool.

Land Use Policy 1.2: Ensure appropriate transitions between uses with different size, scale, and intensity.

- I.2.1 Promote quality design in new development, as well as building orientation, scale, massing, buffering, and setbacks that are appropriate with the context of the surrounding area.

Land Use Policy 1.3: Ensure that development plans incorporate appropriate transportation access and facilities, particularly for bicycle, pedestrian, and transit.

- I.3.1 Require safe, convenient, and direct pedestrian connections between principal building entrances and the public right-of-way in all new development and, where practical, in conjunction with renovation and expansion of existing buildings.
- I.3.2 Ensure the provision of high quality transit, bicycle, and pedestrian access to and within designated land use features.

Land Use Policy 1.15: Support development of Growth Centers as locations for concentration of jobs and housing, and supporting services.

- I.15.2 Support the intensification of jobs in Growth Centers through employment generating development.
- I.15.3 Encourage the development of high- to very high-density housing within Growth Centers.

6. *The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.*

If the requested land use applications are approved, the proposal would comply with all provisions of the B4N Downtown Neighborhood District and the DP Downtown Paring Overlay District.

Findings Required for Planned Unit Developments:

- A. The planned unit development conforms to the applicable standards for alternatives and amenities. All planned unit developments shall provide at least one amenity or a combination of amenities that total at least 10 points, beyond those required for any alternatives. For each alternative requested, amenities shall total at least five points.

Alternatives requested:

Bulk regulations, building height.

In the B4N Downtown Neighborhood District the height limitation at 10 stories or 140 feet. Of the three buildings proposed within the development the residential building exceeds the height limitation as it is proposed to be 17 stories or 185 feet in height. The applicant is proposing to provide five points worth of amenities to allow an increase in the maximum height of the building.

In addition to the conditional use permit standards, the Planning Commission shall consider, but not be limited to, the following factors when determining the maximum height of principal structures in commercial districts:

1. *Access to light and air of surrounding properties.*

Public right-of-way separates the proposed development site from surrounding properties on all sides. Approving the requested increase in height would not impede access to light and air for surrounding properties.

2. *Shadowing of residential properties, significant public spaces, or existing solar energy systems.*

Shadow studies depicting shadowing impacts at three different times of the day during the spring, summer, fall and winter were submitted. The shadow studies indicate that the proposed development will cast shadows on the residential properties to the north and northwest; however, the shadowing impacts do not appear to be significant given the context of the area. The shadow study indicates that due to the fact that the neighborhood is densely built-out, that during the periods of peak shadowing, much of the surrounding vicinity is currently shadowed by existing buildings. In addition, staff is not aware of any existing solar energy systems that would be affected by the proposed development.

3. *The scale and character of surrounding uses.*

The heights of the proposed buildings within the development would be compatible with buildings in the surrounding area which range between one- and 30-stories. The proposed 17-story residential building will be stepped back from South 8th Street starting at the sixth floor and will be stepped back from South 9th Street starting at the ninth floor.

There has been a fair amount of development in the surrounding area over the last decade. Many of the new buildings are tall high-rise buildings that are contemporary in design and have a modern material pallet including brick, metal and glass.

4. *Preservation of views of landmark buildings, significant open spaces or water bodies.*

The development site is not located in an historic district. However, the Ninth Street South Historic District is located across South 9th Street from the site. One of the defining features of the Ninth Street South Historic District is the uniform scale of buildings which range in height from two to four stories. Given this, whether the proposed building was ten stories or 17 stories in height, it would block views of the historic district from the upper levels of the taller high-rise buildings to the north of the development site. It should be noted that the intent of the standard is to preserve public view corridors, not to preserve individual views from private developments. The project will not block views of significant open spaces or water bodies.

On-premise signs.

Signs are subject to the requirements of Chapter 543, On-Premise Signs. In the B4N Downtown Neighborhood District there can be two-and-a-half square foot of signage for every one linear foot of primary building wall. Wall signs are limited to 120 square feet in size. Projecting signs are limited to 48 square feet in size. The height limitation for both wall signs and projecting signs is 28 feet. Freestanding monument signs are limited to 32 square feet in size and can be no taller than 8 feet. The zoning code limits the number of freestanding signs on a zoning lot to one.

The zoning code allows one roof sign that faces a public street on a zoning lot. The following standards apply to roof signs:

- Signs shall be located on buildings that are less than six stories and 84 feet in height.
- Signs shall be internally illuminated or non-illuminated individual letters and/or logo permanently affixed and attached to the parapet wall or building roof and shall face a public street. Roof signs shall not extend outward more than six inches from the structure.
- Signs shall not be located more than five feet from the face of the primary building wall.
- Signs shall be included in the calculation of the total permitted sign area allowed on the primary building wall facing the same street frontage.
- A roof sign may have a vertical dimension of two feet per story, but not more than a total of five feet, above the roof line of a flat roofed building, or the parapet wall, whichever is greater.
- Between sunrise and sunset the maximum luminance shall be 5,000 nits and between sunset and sunrise the maximum luminance shall be 500 nits.

The applicant has not fully developed the sign plan for the development but has indicated that there will be a roof sign on top of the Brewtel that does not meet the standards of the zoning code. The applicant is proposing five points worth of amenities to allow for this exception to the zoning code.

At this time there are no specific sign details (materials, illumination source, installation, etc.). Given this, CPED is not able to evaluate the signs for compliance with the sign adjustment findings. CPED is recommending that the applicant submit a conditional use permit application for an amendment to the planned unit development for signs that includes a master sign plan when there are specific sign plans available. The amenities that will be evaluated as part of this application are intended to apply towards the requested sign alternative. Since CPED is recommending that a new application be submitted when more sign details are known, at least five points worth of amenities from this application shall apply to the new application when it is submitted.

Off-street parking and loading.

The loading space requirement for the development is five large spaces (12 feet by 50 feet) and one small space (10 feet by 25 feet) or four large spaces and three small spaces; one large or two small for the residential building, two large for the hotel, one small for the event center and two large for the

office building. There would be a total of five large loading spaces provided on the site. All of the loading spaces will be located towards the interior of the block. Given the proximity of the loading spaces to each other they can be shared by all of the different uses within the planned unit development. The number of loading spaces proposed within the planned unit development is sufficient. The applicant is seeking an exception to the loading space requirement through the planned unit development. The applicant is not required to provide amenity points for this exception to the zoning code.

Points required for alternatives:

- Establishment of the planned unit development – **10 points.**
- Bulk regulations, height – **5 points.**
- On-premise signs – **5 points.**
- Off-street loading – **0 points.**

Total = 20 points.

Phasing plan. The development will be built in one phase. No future phases of the Kraus-Anderson Block Redevelopment planned unit development are anticipated.

Amenities provided: The applicant is proposing the following amenities from Table 527-1, Amenities:

Underground parking (10 points)

All parking shall be located underground. Where the grade of the site slopes significantly, all parking shall be enclosed in a floor level of the building that does not meet the definition of a story. Further, exterior parking garage walls adjacent to the public street shall not extend more than three (3) feet above the adjacent grade measured from the finished floor of the first level.

All of the parking for the development is located underground and no walls of the parking garage will extend more than three feet above the adjacent grade measured from the finished floor of the first level.

Decorative or pervious surface for on-site parking and loading areas, drives, driveways, and walkways (3 points)

Provide decorative pavers, pervious pavers, stamped concrete, colored concrete, pervious concrete, brick or other decorative or durable materials for a minimum of seventy-five (75) percent of surface parking and/or loading areas, drives aisles, driveways and walkways that comply with the Americans with Disabilities Act accessibility requirements.

More than 75 percent of the hardscape would be composed of stamped, scored or colored concrete. The interior courtyard, drives and walkways would feature a curb-less environment using stamped, scored and colored concrete that differentiates the vehicle driving lane from the pedestrian walkways. There is a total of 35,023 square feet of paving on-site; 77 percent would be stamped, scored or colored concrete and 23 percent would be standard concrete.

Pedestrian improvements (3 points)

A site and building design that allows for exceptional and accessible pedestrian and/or bicycle access through and/or around a site that exceeds the requirements of Chapter 530, Site Plan Review. The improvements shall use a combination of landscaping, decorative materials, access control and lighting to create a safe, clear and aesthetically pleasing access through and/or around the site that complies with the Americans with Disabilities Act accessibility requirements.

The project would significantly improve the existing pedestrian conditions both through and around the site. The public sidewalks would be widened and green boulevards would be added on all four street frontages. In addition, the amount of curb cuts on the block would be reduced from six to three. Each of

the three buildings would have a principal entrance facing a street and the ground floor dwelling units in the residential building would also have individual entrances from the street.

The site would be designed with walkways leading into the interior courtyard from South 8th Street, South 9th Street and 5th Avenue South. The walkways would be differentiated from the vehicle driving lane that leads from 5th Avenue South to South 9th Street through pavement changes. In addition, pedestrian-scaled light fixtures would line the walkways as would decorative planters.

Recycling storage area (1 point)

Provide an easily accessible area that serves the entire building and is dedicated to the collection and storage of non-hazardous materials for recycling, including but not limited to paper, corrugated cardboard, glass, plastics and metals. The recycling storage area shall be located entirely below grade or entirely enclosed within the building.

There would be a recycling storage area located in each of the buildings in the planned unit development. In the residential building, there would be a dedicated recycling chute in the building that would lead to a recycling dumpster on the first floor for comingled materials.

Amenities proposed by the applicant or others (3 points)

The city planning commission may consider other amenities not listed in Table 527-1, Amenities, that are proportionally related to the alternative requested. The commission may assign one (1), three (3), five (5), or ten (10) points based on the proportionality.

LEED for office building (3 points requested)

The applicant is proposing to design the office building so that it meets the minimum standards for LEED Silver certification. The applicant has provided the LEED checklist and documentation to the City, approved by a LEED Accredited Professional (LEED-AP), which shows that the project will comply with LEED Silver requirements.

Table 1. Amenity Points Summary

Amenity	Points Requested by Applicant	Points Recommended by Staff
Underground parking	10 points	10 points
Decorative or pervious surface for on-site parking and loading areas, drives, driveways, and walkways	3 points	3 points
Pedestrian improvements	3 points	3 points
Recycling Storage Area	1 point	1 point
Amenities proposed by the applicant – LEED for office building	3 points	3 points
Total	20 points	20 points

B. The city planning commission may authorize additional uses, subject to the following standards:

The applicant is not proposing to have any uses in the planned unit development that are not otherwise allowed in the B4N Downtown Neighborhood District or the DP Downtown Parking Overlay District.

C. The planned unit development conforms to the required findings for a planned unit development:

1. That the planned unit development complies with all of the requirements and the intent and purpose of this chapter. In making such determination, the following shall be given primary consideration:

- a) The character of the uses in the proposed planned unit development, including in the case of residential uses, the variety of housing types and their relationship to other site elements and to surrounding development.*

The proposed mix of uses within the development include dwellings, a hotel with a restaurant, a brewery, an event center, an office building and underground parking. There would be a mix of studios, one-, two- and three-bedroom units in the residential building. The development would enhance and engage the public realm on all sides with wide sidewalks, green boulevards, multiple entrances and an outdoor interior courtyard.

- b) The traffic generation characteristics of the proposed planned unit development in relation to street capacity, provision of vehicle access, parking and loading areas, pedestrian access, bicycle facilities and availability of transit alternatives.*

Each of the three buildings would have a principal entrance facing a street and the ground floor dwelling units in the residential building would also have individual entrances from the street. The site would be designed with walkways leading into the interior courtyard from South 8th Street, South 9th Street and 5th Avenue South. The walkways would be differentiated from the vehicle driving lane that leads from 5th Avenue South to South 9th Street through pavement changes.

There is no minimum parking requirement for any use in the B4N zoning district. However, in the downtown zoning districts, when residential parking is provided, one guest parking space is required to be provided for every 50 dwelling units within the building. There will be parking provided for the residential portion of the development. There will be 306 dwelling units constructed as part of the development which requires a total of six guest parking spaces. There will be a total of 520 parking spaces provided in two levels of underground parking for all of the uses within the planned unit development.

A Travel Demand Management Plan (TDMP) was completed for the proposed development. The study concludes that, overall, the proposed uses have minimal traffic impacts on intersection operations, all intersections have adequate capacity to accommodate the number of trips that will be generated and, therefore, impacts on existing roadway operations are minimal. In addition, the site has been designed so all truck maneuvering and loading will occur on site.

The applicant will be providing a total of 341 bike parking spaces on the site. There are designated on-street bike routes along Portland Avenue, South 9th Street and 5th Avenue South which tie into the larger bike trail system in the Twin Cities. In addition, there are six Nice Ride Minnesota bike stations located within four blocks of the site.

The site is located within walking distance of several bus routes and the Downtown East Light Rail Transit stop is located five blocks from the site. This site is serviced by both The Blue Line light rail line and The Green Line light rail line.

- c) The site amenities of the proposed planned unit development, including the location and functions of open space, the preservation or restoration of the natural environment or historic features, sustainability and urban design.*

The site amenities include underground parking, decorative or pervious surfaces for on-site parking and loading areas, drives, driveways and walkways, pedestrian improvements, a recycling storage area, and LEED Silver Certification for the office building.

- d) *The appearance and compatibility of individual buildings and parking areas in the proposed planned unit development to other site elements and to surrounding development, including but not limited to building scale and massing, microclimate effects of the development, and protection of views and corridors.*

All three of the buildings would have a strong presence along the street. All three buildings would be located close to the front property lines and would each have a principal entrance facing a street and the ground floor dwelling units in the residential building would also have individual entrances from the street. The height and massing of the three buildings are compatible with one another and with the surrounding buildings. The material palate of each building is simplified and refined. Each is different from one another yet complimentary in tone and material.

The site would be designed with walkways leading into the interior courtyard from South 8th Street, South 9th Street and 5th Avenue South. The walkways would be differentiated from the vehicle driving lane that leads from 5th Avenue South to South 9th Street through pavement changes. In addition, pedestrian-scaled light fixtures would line the walkways as would decorative planters.

- e) *An appropriate transition area shall be provided between the planned unit development and adjacent residential uses or residential zoning that considers landscaping, screening, access to light and air, building massing, and applicable policies of the comprehensive plan and adopted small area plans.*

Public right-of-way separates the proposed development site from surrounding properties on all sides.

- f) *The relation of the proposed planned unit development to existing and proposed public facilities, including but not limited to provision for stormwater runoff and storage, and temporary and permanent erosion control.*

The applicant will be working closely with the Public Works Department, the Plan Review Section of CPED and the various utility companies during the duration of the development to ensure that all procedures are followed in order to comply with city and other applicable requirements. The City of Minneapolis will review and approve the applicant's stormwater management plan and erosion control plan.

- g) *The consideration, where possible, of sustainable building practices during the construction phases and the use of deconstruction services and recycling of materials for the demolition phase.*

Demolition debris will be recycled. In addition, waste generated during construction will also be recycled.

2. *That the planned unit development complies with all of the applicable requirements contained in Chapter 598, Land Subdivision Regulations.*

The applicant is proposing to subdivide the property into three lots; one for each building. The proposed Registered Land Survey will meet the requirements of Chapter 598, Land Subdivision Regulations.

SITE PLAN REVIEW

The Department of Community Planning and Economic Development has analyzed the application based on the required findings and applicable standards in the site plan review chapter:

I. Conformance to all applicable standards of Chapter 530, Site Plan Review.

BUILDING PLACEMENT AND DESIGN

Building placement – Requires alternative compliance

- The residential building would be located between 10 and 18 feet from the front property line along Portland Avenue, two-and-a-half feet from the front property line along South 8th Street and four feet from the corner side property line along South 9th Street. Alternative compliance is required.
- The Brewtel would be located between seven and 12 feet from the front property line along 5th Avenue South and four feet from the corner side property line along South 9th Street. Alternative compliance is required.
- The office building would be located between one and 26 feet from the front property line along 5th Avenue South and between four and 26 feet from the front property line along South 8th Street. Alternative compliance is required.
- The placement of the buildings would reinforce the street walls, maximize natural surveillance and visibility, and facilitate pedestrian access and circulation. The buildings will be set close to the front and corner side property lines given their height and mass, each building will have a principal entrance facing the street and the ground floor units in the residential building would also have individual entrances from the street and there would be large windows on all sides of the development that maximize the opportunities for people to observe adjacent spaces. In addition, the site would be designed with walkways leading into the interior courtyard from South 8th Street, South 9th Street and 5th Avenue South. The walkways would be differentiated from the vehicle driving lane that leads from 5th Avenue South to South 9th Street through pavement changes. In addition, pedestrian-scaled light fixtures would line the walkways as would decorative planters.
- The area between the buildings and the front and corner side property lines include amenities such as landscaping, outdoor dining areas, individual patios, walkways and increased sidewalk width.
- All on-site accessory parking is located entirely below grade.

Principal entrances – Meets requirements

- The buildings are oriented so that at least one principal entrance faces a front property line. The principal to the residential building would be along Portland Avenue and the ground floor units would also have individual entrances from the street. The principal entrance to the Brewtel would be along 5th Avenue South. And the principal entrance to the office building would be along 5th Avenue South.
- All principal entrances to each building are clearly defined and emphasized through the use of entrance canopies, two-story arcades, lots of glass and signage.

Visual interest – Requires alternative compliance

- The building walls provide architectural detail and contain windows in order to create visual interest.
- Each building emphasizes their one unique architectural elements including recesses, projections, windows and window patterns, rooflines, and entries in order to divide themselves into smaller identifiable sections.

- There are no blank, uninterrupted walls exceeding 25 feet in length on the residential building. There are several areas of the Brewtel that are blank and over 25 feet in length. There are segments of wall on the north, south and west sides of the hotel portion of the building that are blank and there are segments of wall on the east and south sides of the brewery portion that are blank. In addition, the east wall of the office building contains areas that are over 25 feet in length and blank. Alternative compliance is required.

Exterior materials – Meets requirements

- The applicant is proposing brick and metal (4 styles/colors) as the residential building's primary exterior materials. Each elevation would comply with the City's durability standards for exterior materials (see the exterior materials breakdown in the attachments). Please note that exterior material changes at a later date may require review by the Planning Commission and an amendment to the site plan review. The exterior materials and appearance of the rear and side walls of the building are similar to and compatible with the front of the building.
- There are different material pallets for the hotel portion and the brewery portion of the Brewtel. The applicant is proposing brick, metal (2 styles/colors) and high pressure laminate as the hotel portion's primary exterior materials and metal (2 styles/colors) and architectural precast wall panels as the brewery portion's primary exterior materials. Each elevation would comply with the City's durability standards for exterior materials (see the exterior materials breakdown in the attachments). Please note that exterior material changes at a later date may require review by the Planning Commission and an amendment to the site plan review. The exterior materials and appearance of the rear and side walls of the building are similar to and compatible with the front of the building.
- The applicant is proposing architectural precast wall panels, metal and stone as the office building's primary exterior materials. Each elevation would comply with the City's durability standards for exterior materials (see the exterior materials breakdown in the attachments). Please note that exterior material changes at a later date may require review by the Planning Commission and an amendment to the site plan review. The exterior materials and appearance of the rear and side walls of the building are similar to and compatible with the front of the building.
- All three of the buildings are in compliance with the City's policy of allowing no more than three exterior materials per elevation, excluding windows, doors, and foundation materials.
- Plain face concrete block is not proposed to be used as an exterior building material along any public street, sidewalk, or adjacent to a residence or office residence district.

Windows – Meets requirements

- For residential uses, the zoning code requires that no less than 20 percent of the walls on the first floor, and no less than ten percent of the walls on each floor above the first that face a public street, public sidewalk, public pathway, or on-site parking lot, shall be windows. The project is in compliance with the minimum window requirement (see Table 3).
- For nonresidential uses, the zoning code requires that no less than 30 percent of the walls on the first floor are windows with clear or lightly tinted glass with a visible light transmittance ratio of six-tenths (0.6) or higher. No less than ten percent of the walls on each floor above the first that face a public street, public sidewalk, public pathway, or on-site parking lot, shall be windows. Based on the floor plans, all proposed shelving, mechanical equipment, and other similar fixtures allow views into and out of the building between four and seven feet above the adjacent grade. The project is in compliance with the minimum window requirement (see Table 3).
- All windows in each building are vertical in proportion and are evenly distributed along the building walls.

Table 2. Percentage of Windows per Applicable Elevation

	Code Requirement		Proposed	
Residential Building				
1st floor facing Portland Avenue	20% minimum	520 sq. ft.	48%	1,250 sq. ft.
2 nd floor and above facing Portland Avenue	10% minimum	374 sq. ft.	More than 10%	
1st floor facing South 8 th Street	20% minimum	114 sq. ft.	35%	198 sq. ft.
2 nd floor and above facing South 8 th Street	10% minimum	82 sq. ft.	More than 10%	
1st floor facing South 9 th Street	20% minimum	114 sq. ft.	28%	158 sq. ft.
2 nd floor and above facing South 9 th Street	10% minimum	82 sq. ft.	More than 10%	
Brewtel				
1st floor facing 5 th Avenue South	30% minimum	425 sq. ft.	41%	583 sq. ft.
2 nd floor and above facing 5 th Avenue South	10% minimum	212 sq. ft.	More than 10%	
1st floor facing South 9 th Street	30% minimum	523 sq. ft.	30%	530 sq. ft.
2 nd floor and above facing South 9 th Street	10% minimum	262 sq. ft.	More than 10%	
Office Building				
1st floor facing 5 th Avenue South	30% minimum	264 sq. ft.	78%	688 sq. ft.
2 nd floor and above facing 5 th Avenue South	10% minimum	154 sq. ft.	More than 10%	
1st floor facing South 8 th Street	30% minimum	473 sq. ft.	67%	1,056 sq. ft.
2 nd floor and above facing South 8 th Street	10% minimum	276 sq. ft.	More than 10%	

Ground floor active functions – Meets requirements

- More than 70 percent of the ground floor in each building facing a public street, public sidewalk, or public walkway contains active functions.

Roof line – Meets requirements

- The principal roof lines of the three buildings will be flat; although the brewery portion of the Brewtel will have a gambrel with a flat peak. Flat roofs are prevalent on buildings in the surrounding area.

Parking garages – Meets requirements

- All of the proposed parking is located below ground.

ACCESS AND CIRCULATION

Pedestrian access – Meets requirements

- There are clear and well-lit walkways at least four feet in width connecting building entrances to the adjacent public sidewalk. In addition, the site would be designed with walkways leading into the interior courtyard from South 8th Street, South 9th Street and 5th Avenue South. The walkways would be differentiated from the vehicle driving lane that leads from 5th Avenue South to South 9th Street through pavement changes.

Transit access – Not applicable

- No transit shelters are proposed as part of this development.

Vehicular access – Meets requirements

- Vehicular access and circulation has been designed to minimize conflicts with pedestrian traffic and with surrounding residential uses.
- The amount of curb cuts on the block would be reduced from six to three. Access to the underground parking would be off of South 8th Street. There would be a one-way drive that leads from 5th Avenue South to South 9th Street. The drive would be used for pedestrian drop-off for the different uses on the block as well as for loading purposes.
- There are no public alleys adjacent to the site.
- Service vehicle access does not conflict with pedestrian traffic. The walkways leading into the interior courtyard would be differentiated from the vehicle driving lane through pavement changes.
- There is no maximum impervious surface requirement in the B4N Downtown Neighborhood District. According to the materials submitted by the applicant, 98 of the site will be impervious, while 92 percent of the existing site is impervious.

LANDSCAPING AND SCREENING

General landscaping and screening – Not applicable

- In the Downtown Districts, any building containing 50,000 square feet or more of gross floor area is exempt from the general landscaping and screening requirements.
- The applicant is proposing to plant four canopy trees, 11 ornamental trees, 165 shrubs and 152 perennials on the site. In addition, the applicant is proposing to plant 25 canopy trees and 285 perennials in the right-of-way.

Table 3. Landscaping and Screening Requirements

	Code Requirement	Proposed
Lot Area	--	109,571 sq. ft.
Building Footprint	--	71,163 sq. ft.
Remaining Lot Area	--	38,408 sq. ft.
Landscaping Required	0 sq. ft.	2,441 sq. ft.
Canopy Trees (1:500 sq. ft.)	0 trees	4 trees
Shrubs (1:100 sq. ft.)	0 shrubs	165 shrubs

Parking and loading landscaping and screening – *Not applicable*

- There is no surface parking proposed for the site, so the site is not subject to the screening and landscaping requirements for parking areas per section 530.170.
- While there will be five designated loading spaces on the site all of them are located towards the interior of the block.

ADDITIONAL STANDARDS

Concrete curbs and wheel stops – *Not applicable*

- There are no surface parking spaces proposed on the site.

Site context – *Meets requirements*

- The development will not block views of important elements of the city such as parks, greenways or water bodies. The Ninth Street South Historic District is located across South 9th Street from the site. One of the defining features of the Ninth Street South Historic District is the uniform scale of buildings which range in height from two to four stories. Given this, the development would block views of the historic district from the upper levels of the taller high-rise buildings to the north of the development site. It should be noted that the intent of the standard is to preserve public view corridors, not to preserve individual views from private developments.
- Shadow studies depicting shadowing impacts at three different times of the day during the spring, summer, fall and winter were submitted. The shadow studies indicate that the proposed development will cast shadows on the residential properties to the north and northwest; however, the shadowing impacts do not appear to be significant given the context of the area. The shadow study indicates that due to the fact that the neighborhood is densely built-out, that during the periods of peak shadowing, much of the surrounding vicinity is currently shadowed by existing buildings.
- The buildings have been designed with stepbacks at different levels, recesses and projections and canopies over the entryways which should help minimize wind effects on the surrounding area.

Crime prevention through environmental design – *Meets requirements*

- The site plan employs best practices to increase natural surveillance and visibility, to control and guide movement on the site, and to distinguish between public and non-public spaces.
- The proposed site, landscaping, and buildings promote natural observation and maximize the opportunities for people to observe adjacent spaces and public sidewalks.
- The project provides lighting on site, at all building entrances, and along walkways that maintains a minimum acceptable level of security while not creating glare or excessive lighting of the site.
- The landscaping, sidewalks, lighting, and building features are located to clearly guide pedestrian movement on or through the site and to control and restrict people to appropriate locations.
- The entrances, exits, signs, fencing, landscaping, and lighting are located to distinguish between public and private areas, to control access, and to guide people coming to and going from the site.

Historic preservation – *Not applicable*

- This site is neither historically designated nor is it located in a historic district.

2. Conformance with all applicable regulations of the zoning ordinance.

The proposed use is conditional in the B4N Downtown Neighborhood District.

Off-street Parking and Loading – Requires conditional use permit

- The applicant is seeking an exception to the loading space requirement through the planned unit development (see Table 7).

Table 4. Vehicle Parking Requirements Per Use (Chapter 54I)

Use	Minimum	Reductions	Total with Reductions	Maximum Allowed	Proposed
Residential Dwellings	0 for dwellings 6 for visitors	--	0 for dwellings 6 for visitors	490	520
Office	0	--	0	107	
Hotel	0	--	0	329	
Limited Production and Processing	0	--	0	8	
Event Center	0	--	0	260	
	6	--	6	1,194	520

Table 5. Bicycle Parking Requirements (Chapter 54I)

Use	Minimum	Short-Term	Long-Term	Proposed
Residential Dwellings	153	--	Not less than 90% 138	306 Total 276 Long-term 30 Short-term
Office	7	--	Not less than 50% 4	12 Total 6 Long-term 6 Short-term
Hotel	0	--	--	3 Total 1 long-term 2 Short-term
Limited Production and Processing	2	--	Not less than 50% 1	8 Total 4 Long-term 4 Short-term
Event Center	0	--	--	12 Total 2 Long-term 10 Short-term
	162	--	143	341

Table 6. Loading Requirements (Chapter 541)

Use	Loading Requirement	Minimum Requirement	Proposed
Residential Dwellings	Low	1 Large or 2 Small	1 Large
Office	DT Requirements	2 Large	2 Large
Hotel	Medium	2 Large	1 Large
Limited Production and Processing	Low	None	None
Event Center	Medium	1 Small	1 Large
	XX	5 Large and 1 Small or 4 Large and 3 Small	5 large

Building Bulk and Height – Requires conditional use permit

- As part of the conditional use permit for the Planned Unit Development the applicant is requesting an increase to the maximum height.

Table 7. Building Bulk and Height Requirements

	Code Requirement	Proposed
Lot Area	--	109,571 sq. ft. / 2.52 acres
Gross Floor Area	--	554,500 sq. ft.
Floor Area Ratio (Minimum)	2.0	5.06
Floor Area Ratio (Maximum)	Not applicable	
Building Height (Maximum)	10 stories or 140 ft., whichever is less	17 ft. or 185 ft.

Lot Requirements – Meets requirements

Table 8. Lot Requirements Summary

	Code Requirement	Proposed
Dwelling Units (DU)	--	306 DUs
Density (DU/acre)	--	121 DU/acre
Lot Area (Minimum)	1 acre	2.52 acres
Impervious Surface Area (Maximum)	Not applicable	Not applicable
Lot Coverage (Maximum)	Not applicable	Not applicable
Lot Width (Maximum)	Not applicable	Not applicable

Yard Requirements – Requires alternative compliance

Table 9. Minimum Yard Requirements

Setback	Zoning District	Overriding Regulations	Total Requirement	Proposed
Front (South 8 th Street)	0 ft.	--	0 ft.	Residential building is 2.5 ft. Office building is between 4 and 26 ft.
Front (Portland Avenue)	0 ft.	--	0 ft.	Residential building is between 10 and 18 ft.
Corner Side (South 9 th Street)	0 ft.	--	0 ft.	Residential building is 4 ft. Brewtel is 4 ft.
Front (5th Avenue South)	0 ft.	--	0 ft.	Brewtel is 4 ft. Office building is between 1 and 26 ft.

Signs – Not applicable

- At this time there are no specific sign details.

Screening of Mechanical Equipment – Meets requirements with Conditions of Approval

- There will be mechanical equipment located on the roofs of each building. The applicant is proposing to screen the rooftop equipment with building materials similar to the exterior materials of the building itself. CPED is recommending that the material meet the requirements of Chapter 535, Regulations of General Applicability.
- There will also be mechanical equipment located on the ground between the residential building and the Brewtel and the office building. The applicant is proposing to screen the mechanical equipment using a metal fence. CPED is recommending that the material meet the requirements of Chapter 535, Regulations of General Applicability.

Refuse Screening – Meets requirements

- There will be a refuse and recycling area located within each building.

Lighting – Choose an item.

- A lighting plan showing footcandles was not submitted as part of the application materials. CPED is recommending that the final lighting plan conform to the standards of Chapter 535, Regulations of General Applicability.

Fences – Meets requirements

- There will be metal fencing located around the mechanical equipment located on the ground between the residential building and the Brewtel and the office building. The proposed fencing meets the standards of Chapter 535, Regulations of General Applicability.

Specific Development Standards – Not applicable

DP Downtown Parking Overlay District Standards – Meets requirements

- The proposal is in compliance with the DP Downtown Parking Overlay District standards. There are no surface parking spaces proposed on the site.

3. Conformance with the applicable policies of The Minneapolis Plan for Sustainable Growth.

The Minneapolis Plan for Sustainable Growth identifies the site as mixed use on the future land use map. The proposed development is consistent with the following principles and policies outlined in the comprehensive plan:

Urban Design Policy 10.1: Promote building designs and heights that enhance and complement the image and form of the Downtown skyline, provide transition to the edges of Downtown and protect the scale and quality in areas of distinctive physical or historical character.

10.1.2 Building placement should preserve and enhance public view corridors that focus attention on natural or built features, such as landmark buildings, significant open spaces or water bodies.

10.1.3 Building placement should allow light and air into the site and surrounding properties.

Urban Design Policy 10.2: Integrate pedestrian scale design features into Downtown site and building designs and infrastructure improvements.

10.2.1 The ground floor of buildings should be occupied by active uses with direct connections to the sidewalk.

10.2.2 The street level of buildings should have windows to allow for clear views into and out of the building.

10.2.3 Ensure that buildings incorporate design elements that eliminate long stretches of blank, inactive building walls such as windows, green walls, architectural details, and murals.

10.2.4 Integrate components in building designs that offer protection to pedestrians, such as awnings and canopies, as a means to encourage pedestrian activity along the street.

10.2.5 Locate access to and egress from parking ramps mid-block and at right angles to minimize disruptions to pedestrian flow at the street level.

10.2.6 Arrange buildings within a site in order to minimize the generation of wind currents at ground level.

10.2.7 Locate buildings so that shadowing on public spaces and adjacent properties is minimized.

10.2.8 Coordinate site designs and public right-of-way improvements to provide adequate sidewalk space for pedestrian movement, street trees, landscaping, street furniture, sidewalk cafes and other elements of active pedestrian areas.

Urban Design Policy 10.10: Support urban design standards that emphasize a traditional urban form in commercial areas.

10.10.1 Enhance the city's commercial districts by encouraging appropriate building forms and designs, historic preservation objectives, site plans that enhance the pedestrian

environment, and by maintaining high quality four season public spaces and infrastructure.

- 10.10.3 Enhance pedestrian and transit-oriented commercial districts with street furniture, street plantings, plazas, water features, public art and improved transit and pedestrian and bicycle amenities.
- 10.10.4 Orient new buildings to the street to foster safe and successful commercial nodes and corridors.
- 10.10.6 Require storefront window transparency to assure both natural surveillance and an inviting pedestrian experience.
- 10.10.7 Encourage the renovation of existing commercial buildings.

CPED finds that the proposed development is in conformance with the above policies of The Minneapolis Plan for Sustainable Growth.

4. Conformance with applicable development plans or objectives adopted by the City Council.

The site is located within the boundaries of the *Elliot Park Neighborhood Master Plan* adopted by the City Council in 2003 and within the boundaries of the *Downtown East/North Loop Small Area Plan* also adopted by the City Council in 2003.

The *Elliot Park Neighborhood Master Plan* identifies the site as mixed use on the future land use map. The plan guidelines say to:

- Create a pedestrian-scaled urban neighborhood with a broad continuum of housing opportunities and sufficient commercial, institutional and recreational facilities to sustain and build community.
- Make Elliot Park a safer, more attractive and more appealing neighborhood.
- Foster a sense of place and community, broaden the mix of uses, improve connectivity, respect architectural form, scale, and context, and reclaim parking lots for in-fill housing and commercial uses.

The *Downtown East/North Loop Small Area Plan* also identifies the site as mixed use on the future land use map. The plan says that development should be between five and 13 stories on the north half of the block and between one and four stories on the south half of the block. The site is located within Development Precinct 8: Downtown Core Expansion. The guidance for this specific area calls for high-intensity office and residential development.

The proposed development is in conformance with the policies of both small area plans.

5. Alternative compliance.

The Planning Commission or zoning administrator may approve alternatives to any site plan review requirement upon finding that the project meets one of three criteria required for alternative compliance. Alternative compliance is requested for the following requirements:

- **Building placement.** The residential building would be located between 10 and 18 feet from the front property line along Portland Avenue, the Brewtel would be located between seven and 12 feet from the front property line along 5th Avenue South and the office building would be located between one and 26 feet from the front property line along 5th Avenue South and between four and 26 feet from the front property line along South 8th Street. Given the height and massing of each building, the greater setbacks from the front property lines are appropriate. In addition, the

greater setbacks allow for widened public sidewalks and additional green space. CPED is recommending that the City Planning Commission grant alternative compliance.

- **Blank walls.** There are several areas of the Brewtel that are blank and over 25 feet in length. There are segments of wall on the north, south and west sides of the hotel portion of the building that are blank and there are segments of wall on the east and south sides of the brewery portion that are blank. In addition, the east wall of the office building contains areas that are over 25 feet in length and blank. Although the buildings both have areas that are blank the mixture of materials and windows tend to mask that fact that the wall segments are over 25 feet in length. The longest portion of the Brewtel that is blank is located towards the interior of the building where both halves face one another. CPED is recommending that the City Planning Commission grant alternative compliance.

REGISTERED LAND SURVEY

The Department of Community Planning and Economic Development has analyzed the application for a Registered Land Survey based on the following findings:

- 1. The subdivision is in conformance with these land subdivision regulations, the applicable regulations of the zoning ordinance and policies of the comprehensive plan.*

The applicant is proposing to vertically subdivide the proposed buildings into different tracts for ownership purposes. Individual lots within a planned unit development are exempt from the public street frontage requirement of section 598.230 and the design requirements of sections 598.240 and 598.250. The subdivision is in conformance with the design requirements of the land subdivision regulations.

Section 598.260 Planned unit developments and cluster design, requires the design of a subdivision for a planned unit development to implement the site plan as approved by the Planning Commission and shall include a deed restriction designating the following:

- a. The relationship between all common spaces and each individual lot (rights in the common spaces and proportionate ownership accruing to the individual lot).*

The applicant will be filing a Reciprocal Easement Agreement that addresses the relationship and rights of the owners of the tracts within the subdivision related to common area, access, maintenance and taxes.

- b. Provision for access to each lot that does not have frontage on a public street.*

The applicant will be filing a Reciprocal Easement Agreement that addresses the relationship and rights of the owners of the tracts within the subdivision related to common area, access, maintenance and taxes.

- c. A requirement that an owner's association be created. The duties and responsibilities of the owner's association shall include maintaining the elements of the planned unit development as authorized under the zoning ordinance or other applicable regulations.*

The applicant will be filing a Reciprocal Easement Agreement that addresses the relationship and rights of the owners of the tracts within the subdivision related to common area, access, maintenance and taxes.

- d. A provision that the taxes, special assessments, and other charges and fees that would normally be levied against the common spaces shall be levied against the individual lot occupied or to be occupied by buildings in direct proportion to the interest that is stated in the deed restriction and shall provide that such levies shall be a lien against the individual lots.*

The applicant will be filing a Reciprocal Easement Agreement that addresses the relationship and rights of the owners of the tracts within the subdivision related to common area, access, maintenance and taxes.

- e. *A requirement that any disposition of any of the common property situated within the planned unit development shall not be made without the prior approval of the Planning Commission.*

If an amendment is made to the Planned Unit Development the proposed amendments shall be reviewed and approved by the City Planning Commission.

2. *The subdivision will not be injurious to the use and enjoyment of other property in the immediate vicinity, nor be detrimental to present and potential surrounding land uses, nor add substantially to congestion in the public streets.*

The proposed subdivision will allow for proposed and future redevelopment of the site. Surrounding uses include a mixture of residential developments of varying densities, hotels, office buildings, commercial establishments and both surface and structured parking facilities. A Travel Demand Management Plan (TDMP) was completed for the proposed development. The study concludes that, overall, the proposed uses have minimal traffic impacts on intersection operations, all intersections have adequate capacity to accommodate the number of trips that will be generated and, therefore, impacts on existing roadway operations are minimal. In addition, the site has been designed so all truck maneuvering and loading will occur on site.

3. *All land intended for building sites can be used safely without endangering the residents or users of the subdivision or the surrounding area because of flooding, erosion, high water table, soil conditions, improper drainage, steep slopes, rock formations, utility easements or other hazard.*

The land intended for building can be used safely without endangering the residents or users of the subdivision or the surrounding area because of flooding, erosion, high water table, soil conditions, improper drainage, steep slopes, rock formations, utility easements or other hazard.

4. *The lot arrangement is such that there will be no foreseeable difficulties, for reasons of topography or other conditions, in securing building permits and in providing driveway access to buildings on such lots from an approved street. Each lot created through subdivision is suitable in its natural state for the proposed use with minimal alteration.*

The tracts created by this application present no foreseeable difficulties for this development. No significant alterations to the land appear necessary.

5. *The subdivision makes adequate provision for stormwater runoff, and temporary and permanent erosion control in accordance with the rules, regulations and standards of the city engineer and the requirements of these land subdivision regulations. To the extent practicable, the amount of stormwater runoff from the site after development will not exceed the amount occurring prior to development.*

The Surface Water and Sewers Division of the Public Works Department have reviewed and approved the drainage and sanitary system plans for this development.

RECOMMENDATIONS

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt staff findings for the applications by Kraus-Anderson for the properties located at 501, 507, 515 and 523 South 8th Street, 502 and 518 South 9th Street and 811 5th Avenue South:

A. Conditional Use Permit for a Planned Unit Development.

Recommended motion: **Approve** the application for a Planned Unit Development, subject to the following conditions:

1. The conditional use permit shall be recorded with Hennepin County as required by Minn. Stat. 462.3595, subd. 4 before building permits may be issued or before the use or activity requiring a conditional use permit may commence. Unless extended by the zoning administrator, the conditional use permit shall expire if it is not recorded within two years of approval.
2. As required by section 527.120 of the zoning code, the development shall comply with the standards for underground parking, decorative or pervious surfaces for on-site parking and loading areas, drives, driveways and walkways, pedestrian improvements, a recycling storage area, and the minimum standards for LEED Silver Certification for the office building from Table 527-1, Amenities totaling a minimum of 20 points.

B. Site Plan Review for a new mixed-use development.

Recommended motion: **Approve** the application for a planned unit development including a 306-unit residential building, a 161-room hotel with a restaurant, a 12,000 square-foot brewery, a 13,000 square-foot event center, a 103,000 square-foot office building for Kraus-Anderson and 520 underground parking spaces, subject to the following conditions:

1. All site improvements shall be completed by March 14, 2018, unless extended by the Zoning Administrator, or the permit may be revoked for non-compliance.
2. CPED staff shall review and approve the final site, elevation, landscaping, and lighting plans before building permits may be issued.
3. All mechanical equipment shall be screened per the requirements of Chapter 535, Regulations of General Applicability.
4. The final lighting plan shall conform to the standards of Chapter 535, Regulations of General Applicability.

C. Registered Land Survey.

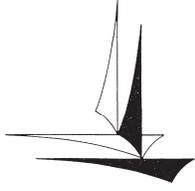
Recommended motion: **Approve** the Registered Land Survey, subject to the following conditions:

1. The Reciprocal Easement Agreement shall be recorded with Hennepin County.
2. This RLS creates tracts that, if separately redeveloped, would not meet the requirements of the zoning and subdivision ordinance for lot frontage on a public street. Approval of this RLS does not constitute approval for the separate redevelopment of any tract if such tract does not comply with applicable subdivision ordinances.

ATTACHMENTS

1. PDR report
2. Written description and findings submitted by applicant
3. Planned unit development application materials
4. Subdivision materials and RLS
5. TDMP
6. Zoning map
7. Exterior materials breakdown
8. Lighting fixture details
9. Civil plans

10. Landscape plans
11. Photos
12. Architectural site plan
13. Renderings
14. Shadow study
15. Floor plans
16. Elevations
17. Correspondence



Minneapolis Development Review
 250 South 4th Street
 Room 300
 Minneapolis, MN 55415

Preliminary Development Review Report

Development Coordinator Assigned: **DONALD ZART**
(612) 673-2726
don.zart@minneapolismn.gov

Status *

Tracking Number:	PDR 1001405	
Applicant:	ENGELSMAN LIMITED PARTNERSHIP 523 8TH ST S MINNEAPOLIS, MN 55404	
Site Address:	523 8TH ST S 501 8TH ST S 507 8TH ST S 515 8TH ST S	502 9TH ST S 811 5TH AVE S 518 9TH ST S
Date Submitted:	03-FEB-2016	
Date Reviewed:		

Purpose

The purpose of the Preliminary Development Review (PDR) is to provide Customers with comments about their proposed development. City personnel, who specialize in various disciplines, review site plans to identify issues and provide feedback to the Customers to assist them in developing their final site plans.

The City of Minneapolis encourages the use of green building techniques. For additional information please check out our green building web page at: http://www.ci.minneapolis.mn.us/mdr/GreenBuildingOptions_home.asp.

DISCLAIMER: *The information in this review is based solely on the preliminary site plan submitted. The comments contained in this report are preliminary ONLY and are subject to modification.*

Project Scope

The proposed redevelopment includes a mix of compatible uses within a composition of high quality individual buildings set within a full block master plan offering several public urban design amenities and benefits:

1. A new Kraus Anderson Corporate Home Office of approximately 100,000 square feet. KA has been located on this site for over 75 years and wishes to retain and expand its presence in Minneapolis for decades to come and help fulfill the Minneapolis 2025 Vision.
2. A new Type 1 residential midrise building of approximately 308 dwelling units featuring innovative design to attract a full range of millennials to baby boomers who want to be part of this new exciting part of Minneapolis and help fulfill the Minneapolis 2025 Vision.
3. A new 161-key boutique hotel that is part of the Finnegan's vision for a boutique

***Approved:** You may continue to the next phase of developing your project.

***Resubmission Required:** You cannot move forward or obtain permits until your plans have been resubmitted and approved.

hotel to support the overall Finnegan's integrated urban brewery / innovation concept.

4. A Finnegan's Brewery, Event Center and FINNovation office space.

Review Findings (by Discipline)

❑ Zoning - Planning

- Please continue to work with staff to identify the required land use applications in preparation of an upcoming City Planning Commission public hearing.

❑ Addressing

- The following are the proposed addresses for the structures:
 - Residence/Apartments - 816 Portland Ave
 - Hotel and Brewery - 823 5th Ave S, since they share an entrance, they should have one street address and then suite numbers inside.
 - Office - 801 5th Ave S
- When assigning suite sequences the following guidelines are as follows:
 - The first one to two digits of the suite sequence number will designate the floor number of the site.
 - The last two digits of the suite sequence number will designate the unique ID for the unit (condo, suite, unit, or apartment).
 - Suite sequence digit numbers will be assigned to dwelling, commercial and retail units, not common areas. For example, laundry rooms, saunas, workout rooms, etc., would not be assigned numbers.
 - Please provide each condo, suite, unit or apartment number.
- This building is also considered to have a parking ramp per MCO Chapter 108. As such, within 5 years of the date of the certificate of occupancy being issued, the parking ramp will be required to have annual inspections and apply for a Ramp Operating Certificate.

❑ Parks - Forestry

- Contact Craig Pinkalla (cpinkalla@minneapolisparks.org), Telephone (612)-499-9233 regarding removal or protection of trees during construction in the city right of way.
- Tree removals approved based on current planting plan for (32) trees in ROW
- Engineered root space with C U Soil meets requirement for all locations
- What is process for opening grate beyond 16" as tree grows?
- Open planting spaces on Portland Ave and 5th Ave need too be 5' wide to approve large tree selections
- or min. 4' wide to approve small tree selections
- Less than 4' (as drawn appears 3') would not be approved for any trees
- unless small variety is planted maximize spacing of two trees in north bed on Portland
- Selections listed in yellow below are represented above diversity goals for this neighborhood
- require alternate selections for Oak and Maple. Hackberry and Linden selections OK
- Species Selection Report will be attached to PDR Report email.
- Effective January 1, 2014, the City of Minneapolis and the Minneapolis Park and Recreation Board adopted an update to the existing Parkland Dedication Ordinance.
- The adopted City of Minneapolis Parkland Dedication ordinance is located in Section 598.340 of the City's Land Subdivision ordinance:
- <http://library.municode.com/index.aspx?clientId=11490>
- As adopted, the fee in lieu of dedication for new residential units is \$1,521 per unit (affordable units excluded per ordinance) and for commercial and industrial development it is \$202.80 per development employee (as defined in ordinance). Any dedication fee (if required) must be paid at the time of building permit issuance. There is also an administration fee that is 5% of the calculated park dedication fee.

- As proposed, for your project, the Kraus Anderson Block Redevelopment, the calculated dedication fee is as follows:
 - Residential - 306 x \$1521 = \$465,426.00
 - Commercial - (Office & Brewery) = \$ 36,504.00
 - Hotel - 161 Rooms = \$ 32,650.80
 - Subtotal = \$534,580.80
 - Administrative Fee- Capped at \$1,000 = \$ 1,000.00
 - Total = \$535,580.80
- This is a preliminary calculation based on your current proposal; a final calculation will be made at the time of building permit submittal.
- For further information, please contact Don Zart at (612)673-2726.

□ Right of Way

- The site plan indicates areas of special treatment of the sidewalk surfaces (colored concrete, joint patterns, etc.); please provide details and notes for these special sidewalk treatments.
- The site plan does not indicate locations of proposed electric utility transformers; it is recommended that the Applicant begin discussions with Xcel Energy as soon as possible in order to identify electric utility and transformer locations on the site plan. It should not be assumed that the City will approve any proposed transformer location in the Public right-of-way.
- An encroachment permit shall be required for all streetscape elements in the Public right-of-way such as: plants & shrubs, planters, tree grates and other landscaping elements, sidewalk furniture (including bike racks and bollards), and sidewalk elements other than standard concrete walkways such as pavers, stairs, raised landings, retaining walls, access ramps, and railings (NOTE: railings may not extend into the sidewalk pedestrian area). Please contact Bob Boblett at (612) 673-2428 for further information.
- Note to the Applicant: Any elements of an earth retention system and related operations (such as construction crane boom swings) that fall within the Public right-of-way will require an encroachment permit application. If there are to be any earth retention systems which will extend outside the property line of the development then a plan must be submitted showing details of the system. All such elements shall be removed from the Public right-of-way following construction with the exception of tie-backs which may remain but must be uncoupled and de-tensioned. Please contact Bob Boblett at (612) 673-2428 for further information.
- In addition, any elements of an earth retention system and related excavations that fall within the Public right-of-way will require a "Right-of-Way Excavation Permit". This permit is typically issued to the General Contractor just prior to the start of construction. However, it is the Applicant's responsibility to insure that all required permits have been acquired by its consultants, contractors, sub-contractor's prior to the start of work.
- Contact Paul Cao at (612) 673-2943 for position and alignment of bike racks proposed in the Public right-of-way. If the racks are privately owned, they will require an encroachment permit.
- The Project limits fall within the boundaries of the Downtown Improvement District (DID). Any improvements, modifications, and alterations to the streetscape are subject to the review and approval of the DID. Please contact Ben Shardlow at (612) 656-3830 for further information.

□ Street Design

- Note to the Applicant: Any currently defective sidewalks or other concrete infrastructure within the public right of way, or any concrete infrastructure damaged during construction, must be removed and replaced.
- The demolition plan provided with the current submittal is unclear as to the extent of the Projects impact to existing Public sidewalks and curb surrounding the site. The Applicant shall provide a demolition plan indicating all planned removals of existing sidewalk and curb & gutter.
- The plan set contains an Architectural Site Plan, but does not provide for an equivalent Civil Site Plan or "paving plan"; the Applicant shall provide a "paving plan" that indicates proposed features such as sidewalk, curb, and driveway aprons.
- All driveway aprons shall be designed and constructed to City standards. All driveway aprons shall be shown graphically correct on all related plan sheets. Please refer to the following:

http://www.minneapolismn.gov/publicworks/plates/public-works_road. Add the following details from the ROAD-2000 Series - Driveways to the plans: ROAD-2000-R1, ROAD-2001-R1, ROAD-2002, and ROAD-2003-R1.

- All curb & gutter in the Public right-of-way shall be designed and constructed to City standards, curb & gutter to be City standard B624 Curb and Gutter. Please refer to the following: http://www.minneapolismn.gov/publicworks/plates/public-works_road. Add the following details from the ROAD-1000 Series - Curbs and Gutters to the plans: ROAD-1003 and ROAD-1010. Top of Curb profiles shall be provided for any section of curb replacement in excess of 50 feet.

□ Sidewalk

- Note to the Applicant: Any currently defective sidewalks or other concrete infrastructure within the public right of way, or any concrete infrastructure damaged during construction, must be removed and replaced.
- The Project streetscape shall be designed in accordance with the Access Minneapolis design guidelines; see <http://www.ci.minneapolis.mn.us/publicworks/transplan/index.htm> for further information. Specifically, the pedestrian zone is of concern, a minimum 6'-0" clear zone (un-obstructed) sidewalk shall be maintained for the length of any block in a straight line (the clear pedestrian zone cannot "jog" around planters and tree grates). Locations for site furniture, tree grates, planters and other proposed design elements that fall within the Public right-of-way shall be modified to provide for the required pedestrian clear zone space.
- The plan set contains an Architectural Site Plan, but does not provide for an equivalent Civil Site Plan or "paving plan"; the Applicant shall provide a "paving plan" that indicates proposed features such as sidewalk, curb, and driveway aprons. For further clarification, site plans shall be fully dimensioned in relation to the property lines, Public right-of-way, sidewalks, street furniture, landscaping, utilities, and other obstructions. The site plan also indicates areas of special treatment of the sidewalk surfaces; please provide details and notes for these special sidewalk treatments.
- Note to the Applicant: 8th St. S. is slated for reconstruction in 2019, and there are future Metro Transit (ABRT) routes planned for the corridor; consequently there may be ABRT Stations along that edge of this block in the future, which could impact the proposed streetscape. It is recommended that the Applicant meet with the appropriate Public Works Engineering staff regarding impacts to the proposed site plan; please contact Paul Miller at (612) 673-3603 for further information.
- ADA compliant pedestrian ramps are required at each impacted crosswalk surrounding the site (recently upgraded ADA compliant pedestrian ramps shall be protected during construction). Construct two (2) ADA compliant pedestrian ramps at each of these locations. All pedestrian ramps will need to meet current ADA regulations and be "Accessible Pedestrian Signal" (APS) ready. Please contact Ryan Anderson at 612-673-3986 for more information on current APS designs. Include the appropriate details and standard plans in the site plan; refer Mn/DOT Standard Plan 5-297.250 Pedestrian Curb Ramp Details at: <http://standardplans.dot.state.mn.us/stdplan.aspx>

□ Traffic and Parking

- A Travel Demand Management Plan (TDMP) was previously submitted, please contact Allen Klugman at 612-673-5750 to discuss potential revisions or amendments to that TDMP based upon the current site plan.
- All street lighting (existing and proposed) shall be shown clearly on the site plan. Prior to site plan approval, the Applicant shall contact Joseph Laurin at 612-673-5987 to determine street lighting requirements. Note: If decorative street lighting exists on the proposed site it must be preserved or replaced at existing levels. Per the City of Minneapolis Street Lighting Policy, street lighting is required as the project falls within the Central Business District. Lighting levels shall be as follows: standard lighting of 1.5 - 2.0 fc (alternating high poles and low poles, approx. 5 on each side of the development).
- All street lighting in the Public right-of-way shall be designed and constructed to City standards. The Applicant shall submit a detailed plan specifying pole locations, light standards and fixture types, and include all required Minneapolis standard plates for installation details. These include design plates for poles (15' aluminum or 30' high level pole), fixture cut sheets from suppliers (must meet full cutoff standard and be a fixture identified in the Minneapolis Street Light Policy), concrete pole foundation details, lighting cabinet (typically 120 volt cabinet CBD for downtown, non-CBD for elsewhere), and wiring diagrams for lighting

connections. A power source for the lighting must be identified from Xcel Energy and be located in the Public right-of-way (street lighting shall not be powered from sources on the private development site).

- Note to the Applicant: In addition to the various required construction permits, impacts to existing traffic signal and street lighting systems (including installation of new street lights) will require the Applicant and respective Contractors to enter into a separate Right-of-Way Excavation Permit (including Testing and Inspection requirements) with the Public Works Department, for further information regarding this permit please coordinate with Paul Miller at (612) 673-3603. If this project chooses not to use City forces to install the street lighting, the Contractor shall follow the standard supplemental specifications for the DIVISION SL-LIGHTING. For detailed information related to City of Minneapolis standard specifications, details, and standard plates refer to the following: <http://www.minneapolismn.gov/publicworks/plates/index.htm>
- The Applicant shall note the location of any existing Metro Transit "bus stops" on the site plan.
- Note to the Applicant: The construction of this development will likely require the use of Public right-of-way (roadway and sidewalks) for construction purposes. A request for an estimate of street use and obstruction permit fees can be made to the City's Traffic Department; please contact Scott Kramer at (612) 673-2383 for further information
- Note to the Applicant: Please add the following notes to the site plan:
 - Street lighting installed as part of the Project shall be inspected by the City. Contractors shall arrange for inspections with the Traffic Department, please contact Dave Prehall at (612) 673-5759 for further information. Any lighting installations not meeting City specifications will be required to be reinstalled at Owner expense.
 - An obstruction permit is required anytime construction work is performed in the Public right-of-way. Please contact Scott Kramer at (612) 673-2383 regarding details of sidewalk and lane closures. Log on to <http://minneapolis.mn.roadway.net/> for a permit.
- Contact Allan Klugman at (612) 673-2743 prior to construction for the temporary removal/temporary relocation of any City of Minneapolis lighting or traffic signal system that may be in the way of construction.
- All costs for relocation and/or repair of City Traffic facilities shall be borne by the Contractor and/or Property Owner.
- Contact Doug Maday at (612) 673-5755 prior to construction for the removal of any City of Minneapolis right of way signs that may be in the way of construction.

❑ Water

- All existing and proposed underground Public Utilities (water, sanitary sewer, and storm drain) shall be shown on the site plan with corresponding pipe sizes and types. For Public watermain infrastructure records call (612) 673-2865. Any existing connections not in use shall be noted on the plans for removal, and shall be removed per the requirements of the Utility Connections Department, call (612) 673-2451 for more information.
- City Ordinance allows for only one (1) domestic and one (1) fire service connection per building. Because of the single underground parking structure; the Water Department considers this to be one (1) large building (similar to Block E) with four above-ground separate structures. Due to the potential of cross connections, in order to protect the City of Minneapolis water supply, this Project will be allowed just one (1) domestic and one (1) fire service connection to feed all the above ground buildings. The Applicant shall contact Rock Rogers at (612) 673-2286 to make arrangements for a meeting with Water Engineering staff to discuss this issue further.
- Meters shall be set at the point where the service line enters the building and shall be set in a location where it is easily assessable. Domestic service lines larger than three inches require a Bypass Assembly (see attachment). Please contact Rock Rogers at (612) 673-2286, to review domestic and fire service design, connections, and sizes.

❑ Fire Safety

- Provide required fire suppression system throughout buildings.
- Fire department connection must be located on the address of building and within 150 feet of a fire hydrant.
- Provide required fire alarm system throughout buildings including all High-Rise requirements.

- Provide & maintain fire apparatus access at all times.

❑ Business Licensing

- Work with Don Zart (612)673-2726 regarding any Health Plan Review submittals that are required for any food related businesses.

❑ Environmental Health

- On the 800 block of 5th Ave N Minneapolis Fire Department records reference a historical gas service station at 811 5th Ave S. MFD records indicate the tanks were abandoned in place in the 1960's. Records have been provided to the Minneapolis Development Coordinator. Building records for the property reference a 1949 electrical permit for wiring of 2 motors and a gas pump.
- If dewatering is required during site construction see below for city permit requirements. The historic high static water level expected for this site should be determined and any subgrade structures used finished above that level. If static water level will be a problem for the current design before installing a continuously operating dewatering system the plan and utility connection must be approved as part of the sanitary sewer and storm drain site plan approval prior to construction beginning.
- No construction, demolition or commercial power maintenance equipment shall be operated within the city between the hours of 6:00 p.m. and 7:00 a.m. on weekdays or during any hours on Saturdays, Sundays and state and federal holidays, except under permit. Contact Environmental Services at 612-673-3867 for permit information.
- Permits and approval are required from Environmental Services for the following activities: Temporary storage of impacted soils on site prior to disposal or reuse; Reuse of impacted soils on site; Dewatering and discharge of accumulated storm water or ground water, underground or aboveground tank installation or removal, well construction or sealing. Contact Tom Frame at 612-673-5807 for permit applications and approvals.
- A review of the project, permits issued and an inspection from Environmental Service for identification of equipment and site operations that require annual registration with the City of Minneapolis will occur for this project.

❑ Historical Preservation Committee

- The wrecking of the existing structure requires a historic review as part of the wrecking permit process. This permit is required to be submitted by a licensed wrecking contractor.

❑ Sewer Design

Groundwater:

- Please identify the lowest floor elevations of the buildings on the grading plan. Please also provide a copy of any geotechnical reports for the site and indicate if any groundwater discharges are proposed in order to keep the below grade portions of the building dry.

Stormwater Management:

- Please provide the necessary documentation and modeling demonstrating that the site is achieving the City's rate control requirements.
- Please provide copies of any certifications for the proposed Contech Stormfilter system demonstrating the product achieves a minimum of 70% TSS removal.
- An operations and maintenance plan is required for the stormwater treatment devices. The O&M plan shall define the maintenance regimen, including type and interval of maintenance and party to conduct such maintenance. Please provide a copy of the O&M Plan.
- The shared stormwater management system and infrastructure will require private easements and/or agreements between the properties. These easements and/or agreements will need to be recorded with the County. Please provide a copy of these.

Utility Connections:

- The proposed storm sewer service connections to the City mains in Portland and 5th Ave should be core-drilled and a saddle tee fitting installed, per City of Minneapolis Standard Supplemental Specifications. A new manhole is not permitted for the connection. Please revise the plans accordingly.
- It is preferred that the proposed sanitary sewer connections be made at the existing manholes upstream or downstream of each of the current proposed connection locations. Please evaluate if this is feasible, if not please contact Jeremy Strehlo, 612-673-3973, for further requirements for the proposed connection directly to the main.
- Please justify the proposed sanitary sewer service connection sizes. Four 10" sanitary sewer service connections seems excessively large. The service sizes should be minimized as much as possible, while providing sufficient capacity, to reduce maintenance issues.

Utility:

- Several notes containing the invert elevation of the City storm and sanitary sewer structures have been cut off on the utility and drainage plan. This information should show up on those plan sheets.
- For comments or questions on Public Works Surface Water & Sewers Division related requirements please contact Jeremy Strehlo, (Professional Engineer) at (612) 673-3973, or jeremy.strehlo@minneapolismn.gov

❑ Construction Code Services

- Contact the Met Council regarding a SAC determination for all the proposed structures. For more information see this link.
- http://www.ci.minneapolis.mn.us/www/groups/public/@regservices/documents/webcontent/convert_281675.pdf

END OF REPORT

KRAUS ANDERSON BLOCK REDEVELOPMENT
STATEMENT OF PURPOSE AND DESCRIPTION OF PROJECT
January 29, 2016

I. DESCRIPTION OF THE PROJECT

A. In General

The vision of the proposed KA redevelopment is to transform an entire city block, consisting of a surface parking lot and antiquated office building, into a vibrant, high-density mixed use destination in East Downtown. This area of East Downtown is currently characterized by several large surface parking lots and is in need of creative infill redevelopment. The proposed project can also serve as a catalyst for future improvements and investment along the Portland Avenue Corridor.

The proposed redevelopment includes a mix of compatible uses within a composition of high quality individual buildings set within a full block master plan offering several public urban design amenities and benefits:

1. A new Kraus-Anderson corporate home office of approximately 100,000 square feet. KA has been located on this site for over 75 years and wishes to retain and expand its presence in Minneapolis for decades to come and help fulfill the Minneapolis 2025 Vision.
2. A new Type 1 residential midrise building of approximately 308 dwelling units featuring innovative design to attract a full range of millennials to baby boomers who want to be part of this new exciting part of Minneapolis and help fulfill the Minneapolis 2025 Vision.
3. A new 161-key boutique hotel that is part of the Finnegan's vision for a boutique hotel to support the overall Finnegan's integrated urban brewery / innovation concept.
4. A Finnegan's Brewery, Event Center and FINNOvation office space.
 - a. The FINNOvation Lab has three exciting elements:
 - i. A Collaborative Community for Social Entrepreneurs: The FINNOvation Lab will be a community of companies with revenue generating, sustainable business models that create living wage jobs in the community and provide positive social impact.
 - ii. For Businesses That Do Well and Do Good: The FINNOvation Lab will cater specifically to social entrepreneurs and innovators – businesses that do well and do good. It will provide a tailored ecosystem of support services from the start up to scaling phase of their business.

- iii. Designed To Enhance Collaboration And Cross Pollination Of Ideas: The Lab space will be purposely designed to enhance collaboration and cross pollination of ideas among the diverse individuals, businesses and organizations that become a part of the lab community. It will be a hub for those passionate about social innovation, creativity and collaboration.
- b. Finnegan House is a destination development, combining the rapidly growing trends of the craft beer movement, collaborative workspaces, social entrepreneurship and experiential overnight accommodations to create a positive impact on the community and greater society. Finnegans will be the first beer company in the world to donate 100 percent of its profits back to the community, and will launch its first ever brewery and experiential brewery lodging experience with 161 guest rooms at Finnegan House. This community building space will also include an outdoor courtyard and event space with planned programming and events designed to engage the neighborhood and the greater community. A restaurant and bar will be located inside Finnegan House, to provide food and beverage services to the entire facility, including catering and room service.

B. Key Overall Design Features and Public Benefits

1. Public Realm and Streetscape:

- a. Complete rebuilding and enhancements to the surrounding urban sidewalks at Portland Avenue, 9th Street, 5th Avenue and 8th Street.
- b. All four sidewalk areas will feature green boulevards with street trees, new regular and decorative sidewalk pavement, street lighting, landscaping, and outdoor seating areas.
- c. Additional setbacks to provide a wider, more pedestrian friendly environment.
- d. Pedestrian access through site: The site plan features exterior pedestrian access through the site at street level by means of access/egress passages along 9th Street, 8th Street, and 5th Avenue.
- e. Multipurpose service/drop off courtyard: Within the site will be a multi-purpose hardscape courtyard that can be used by the surrounding new development for staging a variety of activities, gatherings and outdoor dining.

2. Parking Strategy: All parking will be provided below street level in a two-level parking facility with limited access located off of 8th Street South. There will be no surface parking lots. Parking for the hotel and Innovation Center will be provided through an offset, shared parking strategy.

3. Architectural Design:

- a. Architectural design approach: The overall architectural strategy features three distinctly different building aesthetics within a harmonious overall block massing approach. The visual intent is the creation of a full block redevelopment that has grown organically over time. Type 1, 100-year concrete and steel construction will be used throughout.
- b. Streetscape public realm shaped by high density/active architecture: The architecture on all sides of the block, shaping the streetscape public realm, will feature a variety of high quality, engaging facades, multiple entry points, and significant expanses of transparent window area to create an active and comfortable public pedestrian environment.

4. Stormwater Management: The stormwater management design will follow City of Minneapolis best practices.

5. Green And Sustainable Features: The design will offer a pedestrian and transit-oriented community that allows residents to live, work and play without the dependence on daily automobile usage. The development team is committed to the sustainable design principles reflected in the City's Comprehensive Plan. Our sustainable design mission is to promote livable communities through the use of energy efficient systems, green building practice, reduced dependency on automobiles, creative density, high quality pedestrian and bicycle public realm and preservation of natural resources. This mixed use project will feature a series of green elements including green construction and demolition practices, green material specifications, thermal high-efficiency windows, and exterior envelope systems, and participation in the Xcel Energy Design Assistance Program.

II. LAND USE APPLICATIONS

To the best of our knowledge, the applications required for the project are:

- A. Planned Unit Development ("PUD");
- B. Conditional use permit for height;
- C. Site plan review;
- D. Registered Land Survey; and
- E. Signage (as part of PUD application).

SPECIFIC APPLICATION REQUIREMENTS (SEE APPLICATION INSTRUCTIONS PAGES 5-8).

ALL CONDITIONAL USE PERMITS: A written statement by the applicant which addresses the following required findings:

1) The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety comfort or general welfare.

The proposed development will not be detrimental to or endanger the public health, safety, comfort or general welfare, and granting the CUP will allow a development that will benefit the neighborhood and reinforce goals of the comprehensive and small area plans. Infill development on this site will have a positive effect on the health, safety and vitality of the area. The new construction will fill a hole in the urban fabric of the neighborhood with beautiful buildings of high architectural quality. The building and site design and the addition of commercial uses on this block will activate the pedestrian realm. New commercial uses and the addition of residents will enhance the East Downtown area. The new construction will comply with all building and site development codes.

2) The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property of uses permitted in the district.

This mixed use development will not be injurious to the use and enjoyment of other property or impede development and improvement of surrounding property. The residential components are similar to other mid- and high-rise development in the area. The street-level commercial development will expand and complement the East Downtown area. High-density, mixed use redevelopment of this site is consistent with City goals and, by being responsive to the land use policies for this area, the project will promote the orderly development of the neighborhood.

3) Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.

Adequate utilities, access, drainage and other facilities will be provided. The development team will continue to work closely with Public Works, Plan Review and Planning staff to comply with City and other applicable requirements.

The City of Minneapolis requires that all new developments meet the stormwater management requirements of Chapter 54 of the City Code of Ordinances in order to be approved for construction. These requirements consist of a reduction in the runoff rate for the 2-year, 10-year, and 100-year, 24 hour storm events, and the removal of 70% of total suspended solids which occur during the 1-1/4" rainfall event.

4) Adequate measures have been or will be taken to minimize traffic congestion in the public streets.

A traffic impact study and Travel Demand Management Plan were prepared in conjunction with an Environmental Assessment Worksheet that evaluated the potential impacts of redevelopment of the site. With implementation of the improvements recommended by the study, traffic operations under full-build conditions are expected to operate acceptably. The TDMP also outlines the ways in which the development reduces vehicular trips and helps Minneapolis achieve its goals of enhancing the local transportation system.

All loading will occur on site in the service drive. The parking garage will provide all parking required for the residential and commercial uses.

5) The conditional use is consistent with the applicable policies of the comprehensive plan.

The project site is included in two land use plans: the Minneapolis Plan for Sustainable Growth, which is the Citywide comprehensive plan and the Elliot Park Downtown District. Both of these plans identify the future land use for the project site as mixed use. This land use category allows for mixed use development, including mixed use with residential. Mixed use may include either a mix of retail, office or residential uses within a building or within a district.

The following land use policies and implementation steps from the Minneapolis Plan for Sustainable Growth apply to the proposal:

Land Use Policy 1.4: Develop and maintain strong and successful commercial and mixed use areas with a wide range of character and functions to serve the needs of current and future users.

Land Use Policy 1.5: Promote growth and encourage overall city vitality by directing new commercial and mixed use development to designated corridors and districts.

Land Use Policy 1.8: Preserve the stability and diversity of the city's neighborhoods while allowing for increased density in order to attract and retain long-term residents and businesses.

1.8.1 Promote a range of housing types and residential densities, with highest density development concentrated in and along appropriate land use features.

Land Use Policy 1.9: Through attention to the mix and intensity of land uses and transit service, the City will support development along Community Corridors that enhances residential livability and pedestrian access.

1.9.3 Discourage uses that diminish the transit and pedestrian oriented character of Community Corridors, such as automobile services and drive-through facilities.

1.9.6 Promote more intensive residential development along Community Corridors near intersections with Neighborhood Commercial Nodes and other locations where it is compatible with existing character.

6) The conditional use shall in all other respects, conform to the applicable regulations of the district in which it is located.

Upon approval of the submitted applications, the project will conform with the applicable regulations of the B4N zoning district.

INCREASING MAXIMUM HEIGHT:

1. Access to light and air of surrounding properties.

Public right-of-way, and generous setbacks separate the proposed development from surrounding properties. Allowing the requested increases in height will not impede access to light and air for the surrounding properties.

2. Shadowing of residential properties, significant public spaces, or existing solar energy systems.

A shadow study has been submitted that shows the degree of shadowing of other residential properties by the project. Like other existing and new residential towers in the area, the project will cast shadows at certain times of the day and year, but the effects are mitigated by the north/south compass orientation of the proposed 17 story residential midrise, and the fact that the PUD site is surrounded by public streets. The project does not shadow any significant public spaces. No existing solar energy systems are known to be shadowed by the project.

3. The scale and character of surrounding uses.

The scale and character of the buildings in the project is compatible with surrounding uses and consistent with the design guidance in the Elliot Park Downtown District Guidelines. The overall design massing of the proposed design steps down and transitions from the adjacent surrounding mid and high-rise buildings into the historic Elliot Park neighborhood south of 9th street.

4. Preservation of views of landmark buildings, significant open spaces or water bodies.

The project will not block views of landmark buildings, significant open spaces or water bodies.

SIGN ADJUSTMENTS:

1. The sign adjustment will not significantly increase or lead to sign clutter in the area or result in a sign that is inconsistent with the purpose of the zoning district in which the property is located.

TBD

2. The sign adjustment will allow a sign that relates in size, shape, materials, color, illumination and character to the function and architectural character of the building or property on which the sign will be located.

TBD

PLANNED UNIT DEVELOPMENT:

Submit the required items found in the Planned Unit Development Application Packet.

See PUD application.

OTHER CONDITIONAL USE PERMITS:

See PUD application.

VARIANCE:

SIGN VARIANCE:

3. The sign adjustment will not significantly increase or lead to sign clutter in the area or result in a sign that is inconsistent with the purpose of the zoning district in which the property is located.

TBD

4. The sign adjustment will allow a sign that relates in size, shape, materials, color, illumination and character to the function and architectural character of the building or property on which the sign will be located.

TBD

RESPONSES TO THE SECTION 527.260 FINDINGS FOR A PLANNED UNIT DEVELOPMENT.

In addition to the CUP standards, the project complies with the additional findings required for planned unit developments.

1) That the planned unit development complies with all of the requirements and the intent and purpose of the PUD chapter. In making such determination, the following shall be given primary consideration:

- a. The character of the uses in the proposed planned unit development, including in the case of residential uses, the variety of housing types and their relationship to other site elements and to surrounding development.** The overall development is designed to fit into its context. The office building is located closest to the downtown office core, the hotel and brewery/event center is located along 5th street opposite an existing hotel, and the residential tower is sited to relate to the existing residential district of Elliot Park. The residential tower is stepped along Portland Avenue, specifically to step down toward the 9th Street Historic District. The residential tower includes walk up dwelling units on Portland Avenue and 9th Street that relate to similar historic multi-family residences in the historic district, as well as more recent townhome dwelling units at Grant Park two blocks away. The intent of this variety along Portland Avenue is to help build another part of the Portland Avenue Residential Corridor as a highly landscaped, pedestrian friendly zone.
- b. The traffic generation characteristics of the proposed planned unit development in relation to street capacity, provisions of vehicle access, parking and loading areas, pedestrian access, bicycle facilities and availability of transit alternatives.** The traffic impact study concludes that, with certain mitigation and improvements, the existing street capacity is adequate to support the traffic that will be generated by the project. All required parking will be provided in the parking garage and loading will occur on-site. Bicycle storage and repair areas will also be provided in the garage and will have convenient access to the street. The streetscape design integrates with the adjacent bus stops.
- c. The site amenities of the proposed planned unit development, including the location and functions of open space, the preservation or restoration of the natural environment or historic features, sustainability and urban design.** Site amenities of the proposed PUD include a significant setback of the residential building along Portland Avenue in order to create a richly landscaped, pedestrian zone. Sidewalks at the perimeter of the site currently abut the back of curbs. The new project will create several green boulevards planted with full over-story trees along much of the perimeter of the site in order to increase pervious surfaces. All vehicular parking is being moved below grade and the interior of the block is

being designed as a highly flexible service-amenity court. This open space will be defined by the buildings around it and is open to pedestrian movement through the site at three locations. This urban courtyard area will become a more intensely used space for both service functions and amenity functions as opposed to the current single use parking lot.

- d. The appearance and compatibility of individual buildings and parking areas in the proposed planned unit development to other site elements and to surrounding development, including but not limited to building scale and massing, microclimate effects of the development and protection of view and corridors.** The scale and design of the buildings are compatible with other mid-rise and tower buildings on nearby blocks.
- e. An appropriate transition area shall be provided between the planned unit development and adjacent residential uses or residential zoning that considers landscaping, screening, access to light and air, building massing and applicable policies of the comprehensive plan and adopted small area plans.** Appropriate transitions between the project and adjacent residences are provided by stepping down the massing and incorporation of walk-up units along Portland Avenue. The project will add landscaping and trees along the now-barren street fronts. The buildings are setback from the streets to create an expanded public realm in accordance with the goals of Elliot Park and East Downtown.
- f. The relation of the proposed planned unit development to existing and proposed public facilities, including but not limited to provision for stormwater runoff and storage, and temporary and permanent erosion control.** As discussed above in proposed finding #3, the project will comply with all requirements for stormwater management and erosion control. Existing public facilities will be utilized.
- g. The consideration, where possible, of sustainable building practices during the construction phases and the use of deconstruction services and recycling of materials for the demolition phase.** The development team is committed to the sustainable design principles reflected in the City's comprehensive plan. Our sustainable design mission is to promote livable communities through the use of energy efficient systems, green building practices, reduced dependency on automobiles, creative density, high quality pedestrian and bicycle public realm and preservation of natural resources.

PLANNED UNIT DEVELOPMENT APPLICATION WORKSHEETS

In the table below, identify the requested alternative(s) and determine the minimum points required.

AUTHORIZED ALTERNATIVES		MARK AN 'X' NEXT TO THE REQUESTED ALTERNATIVE(S)	POINTS REQUIRED FOR THE REQUESTED ALTERNATIVE
Number of principal residential structures.	To allow more than one (1) principal residential structure to be placed on one (1) platted or recorded lot.	<input checked="" type="checkbox"/>	0
Bulk regulations.	To increase the maximum gross floor area ratio by not more than twenty (20) percent.	<input type="checkbox"/>	5
	To increase the maximum height of structures.	<input checked="" type="checkbox"/>	
Lot area requirements.	Reductions in the area of individual lots within the planned unit development from the required lot area of the zoning district.	<input type="checkbox"/>	5
	A density bonus to increase the maximum number of dwelling units by not more than twenty (20) percent.	<input type="checkbox"/>	
Yards.	A reduction or elimination of required yards within the planned unit development.	<input type="checkbox"/>	5
	A reduction or elimination of required yards along the periphery of the planned unit development, except along property lines abutting a side or rear lot line of a residential district or property lines abutting a side or rear lot line of a structure used for permitted or conditional residential purposes.	<input type="checkbox"/>	
Building Placement.	Allow alternatives to building placement requirements in the PO Pedestrian Oriented Overlay District, except where to allow parking between the principal structure and the front or corner side lot line.	<input type="checkbox"/>	5
On-premise signs.	Sign standards.	<input checked="" type="checkbox"/>	5
Off-street parking and loading.	Minimum and maximum amount of required off-street parking and loading.	<input checked="" type="checkbox"/>	0
	Minimum amount of required bicycle parking.	<input type="checkbox"/>	
	Minimum width of parking aisles.	<input type="checkbox"/>	
	Minimum and maximum width of driveways.	<input type="checkbox"/>	
Amenity(ies) required by section 527.120(1)		<input checked="" type="checkbox"/>	10
Add points required for each requested alternative to determine total points required. Enter total at right in the space provided.			20

In the table below, identify the proposed amenity(ies) and determine the amount of points proposed. An amenity(ies) used to qualify for a density bonus in any zoning district, a floor area ratio premium in the Downtown Districts, or any other amenity in Table 527-1 cannot be counted as an amenity for a requested alternative. No points are awarded for amenities that do not meet all of the standards required in Table 527-1, unless otherwise authorized by the city planning commission.

AMENITY	MARK AN 'X' NEXT TO THE PROPOSED AMENITY(IES)	POINTS
Active liner uses as part of a parking garage	<input type="checkbox"/>	10
Green roof	<input type="checkbox"/>	10
Historic preservation	<input type="checkbox"/>	10
Leadership in Energy and Environmental Design (LEED)	<input type="checkbox"/>	10
Minnesota Sustainable Building Guidelines (B3-MSBG)	<input type="checkbox"/>	10
Public right-of-way dedication	<input type="checkbox"/>	10
Underground parking	<input checked="" type="checkbox"/>	10
Conservation of the built environment	<input type="checkbox"/>	5
Garden(s) or on-site food production	<input type="checkbox"/>	5
On-site renewable energy	<input type="checkbox"/>	5
Outdoor open space	<input type="checkbox"/>	5
Outdoor children's play area	<input type="checkbox"/>	5
Plaza	<input type="checkbox"/>	5
Art feature	<input type="checkbox"/>	3
Decorative or pervious surface for on-site parking and loading areas, drives, driveways and walkways.	<input checked="" type="checkbox"/>	3
Energy efficiency	<input type="checkbox"/>	3
Living wall system	<input type="checkbox"/>	3
Natural features	<input type="checkbox"/>	3
Pedestrian improvements	<input checked="" type="checkbox"/>	3
Reflective Roof	<input type="checkbox"/>	3
Shared bicycles	<input type="checkbox"/>	3
Shared vehicles	<input type="checkbox"/>	3
Decorative fencing	<input type="checkbox"/>	1

Enhanced exterior lighting	<input type="checkbox"/>	1
Enhanced landscaping	<input type="checkbox"/>	1
Enhanced stormwater management	<input type="checkbox"/>	1
Heated drives or sidewalks	<input type="checkbox"/>	1
Pet exercise area	<input type="checkbox"/>	1
Recycling storage area	<input checked="" type="checkbox"/>	1
Tree islands	<input type="checkbox"/>	1
Water feature	<input type="checkbox"/>	1
Amenities proposed by the applicant or others:	<input checked="" type="checkbox"/>	1, 3, 5, or 10, as determined by CUP.
Amenities that significantly exceed standards:	<input type="checkbox"/>	Up to 5, as determined by CUP.
Add points for each proposed alternative to determine total points proposed. Enter total at right in the space provided.		20

INFORMATION, PLANS, ETC. SHOWING STANDARDS FOR HOW PROPOSED AMENITIES WILL MEET THE REQUIRED STANDARDS.

Underground Parking (10 points)

All parking shall be located underground. Where the grade of the site slopes significantly, all parking shall be enclosed in a floor level of the building that does not meet the definition of a story. Further, exterior garage walls adjacent to the public street shall not extend more than three (3) feet above the adjacent grade measured from the finished floor level.

All parking for the project will be provided below grade and will not extend more than three feet above the adjacent grade as measured from the finish floor level. The entrance and exit to the below grade parking occurs mid-block on 8th Street. The at grade courtyard is to be utilized by vehicles only for service and drop-off functions.

Decorative or pervious surface for on-site parking and loading areas, drives, driveways and walkways (3 Points)

Provide decorative pavers, pervious pavers, stamped concrete, colored concrete, pervious concrete, brick or other decorative or durable materials for a minimum of seventy five (75) percent of surface parking and/or loading areas, drives aisles, driveways and walkways that comply with the Americans with Disabilities Act accessibility requirements.

More than 75% of the hardscape interior of the site will be composed of stamped, scored or colored concrete. The interior courtyard, drives and walkways will feature a curb-less environment featuring stamped, scored and colored concrete that imply the path for vehicular movement and the zone for pedestrian movement. See attached civil and landscape plans for design details.

Colored Concrete Paving: 26,944 sq ft. (77%)

Standard Concrete Paving: 8,079 sq ft. (23%)

Total Site Paving (not including public right of way sidewalks) = 35,023 sq ft.

Pedestrian Improvements (3 Points)

A site and building design that allows for exceptional and accessible pedestrian and/or bicycle access through and/or around a site that exceeds the requirements of Chapter 530, Site Plan Review:

The site plan provides for accessible pedestrian connections through the site at three points: on 5th Avenue S. between the KA Office Building and the Brewery, on 9th Street between the Hotel and the Residential Building, and on 8th Street between the Office

Building and the Residential Building. At these three locations, pedestrians have access to, and through, the interior of the site where the Service / Amenity Courtyard is located. At the perimeter of the site, a planted boulevard will be installed between the street and public sidewalk which doesn't exist currently. Along Portland Avenue and 9th Street, ground floor dwelling units will have direct access to the public sidewalk as well.

The improvements shall use a combination of landscaping, decorative materials, access control and lighting to create a safe, clear and aesthetically pleasing access through and/or around the site that complies with the Americans with Disabilities Act accessibility requirements.

75% of the hardscape sidewalks within the site will be made of scored and colored concrete. The interior courtyard will be a curb-less environment that uses scored and colored concrete paving to indicate pedestrian and vehicular zones, and provides for ease of use per ADA requirements. The amenity area that is programmed by the hotel / brewery will also be designed with colored and scored concrete paving. Four large above grade planters will help delineate this amenity area and provide for greenery in the courtyard. The hotel drop off will feature scored and colored concrete paving indicating the pathway into the hotel and brewery complex. The hotel and residential lobbies will be lit 24 hours per day, and additional lighting bollards and decorative lighting will create a safe and pleasing environment in the courtyard area at all hours of the day.

Recycling storage area (1 Point)

Provide an easily accessible area that serves the entire building and is dedicated to the collection and storage of non-hazardous materials for recycling, including but not limited to paper, corrugated cardboard, glass, plastics and metals. The recycling storage area shall be located entirely below grade or entirely enclosed within the building.

There will be a recycling storage area provided in each of the buildings on the site. The recyclable materials will be collected in bins located inside each building. Each of the recycling areas is located near an elevator for easy access. The residential building will have a dedicated recycling chute with a dedicated recycling dumpster on the first floor that collects comingled recycling. Each trash/recycling room meets the area requirements of the zoning code.

FOR AN AMENITY NOT SPECIFICALLY LISTED IN TABLE 527-1 PROPOSED BY THE APPLICANT, A WRITTEN DESCRIPTION, PROVIDE INFORMATION, PLANS, ETC TO DESCRIBE THE PROPOSED AMENITY.

LEED for Office Building (amenity proposed by applicant) (3 Points)

The proposed KA Office Building shall meet the minimum standards for LEED Silver certification. The project does not have to achieve actual LEED certification; however, the developer must submit the LEED checklist and documentation to the City, approved by a LEED Accredited Professional (LEED-AP), that shows the project will comply with LEED Silver requirements. Kraus Anderson's new corporate office is currently registered for, and will seek, Silver LEED certification under the USGBC's LEED NC v 2009 Platform. There are several sustainable design strategies that are incorporated into the design of their building to obtain this certification. Below are highlights of these strategies and attached as **Exhibit A** is the LEED checklist with the full list of credits Kraus Anderson will be seeking at this time.

Among the sustainable site strategies are access to public transportation, the inclusion of bicycle storage and changing/shower rooms to promote a reduction in automobile use. Also important to the site is the filtration of stormwater run-off and the reduction of light pollution from artificial building interior and exterior lighting sources.

In regards to water efficiency, Kraus Anderson is seeking to reduce indoor water consumption for 30% over a baseline office building through the use of low flow and automatic plumbing fixtures.

Within the Energy and Atmosphere LEED category, Kraus Anderson's new office will employ energy efficient measures in order to be a minimum of 10% more efficient than code requires. They have also enlisted an independent commissioning agent to review and assure that their mechanical systems are running accurately.

Materials and Resources have been specified to assure that regional materials and high recycled building materials are used throughout the exterior and interior of the building. They have also put into place an internal recycling program for staff and have established construction waste policies for use with recycling of construction materials such that a minimum of 75% of all construction materials will be recycled.

Indoor Air Quality is critical to Kraus Anderson to provide a healthy interior building for staff and occupants. Low VOC paints, coatings, flooring, adhesives, sealants and wood products will be used throughout the interior space. An Indoor Air Quality Plan has been set in place for both during construction and for post-construction/pre-occupancy.

Attached is the LEED checklist that outlines all of the sustainable LEED strategies that Kraus Anderson will be seeking for their new building.

EXHIBIT A

LEED Checklist

LEED 2009 for New Construction and Major Renovations

Project Checklist

Revised 9.04.14

Y	1	6	Possible Points:	26
Sustainable Sites				
Y	1	1	Construction Activity Pollution Prevention	1
			Site Selection	1
	5	1	Development Density and Community Connectivity	5
			Brownfield Redevelopment	1
	6	1	Alternative Transportation—Public Transportation Access	6
			Alternative Transportation—Bicycle Storage and Changing Rooms	1
	3	1	Alternative Transportation—Low-Emitting and Fuel-Efficient Vehicles	3
			Alternative Transportation—Parking Capacity	2
			Site Development—Protect or Restore Habitat	1
			Site Development—Maximize Open Space	1
			Stormwater Design—Quantity Control	1
			Stormwater Design—Quality Control	1
			Heat Island Effect—Non-roof	1
			Heat Island Effect—Roof	1
			Light pollution Reduction	1
4	4	4	Water Efficiency	10
Y	2	2	Water Use Reduction—20% Reduction	2 to 4
			Water Efficient Landscaping	2
	2	2	Innovative Wastewater Technologies	2 to 4
			Water Use Reduction	2
8	4	23	Energy and Atmosphere	35
Y			Fundamental Commissioning of Building Energy Systems	
			Minimum Energy Performance	
			Fundamental Refrigerant Management	
	4	15	Optimize Energy Performance	1 to 19
			On-Site Renewable Energy	1 to 7
	2	6	Enhanced Commissioning	2
			Enhanced Refrigerant Management	2
			Measurement and Verification	3
			Green Power	2
5	1	8	Materials and Resources	14
Y			Storage and Collection of Recyclables	
	3	1.1	Building Reuse—Maintain Existing Walls, Floors, and Roof	1 to 3
			Building Reuse—Maintain 50% of Interior Non-Structural Elements	1
	2	2	Construction Waste Management	1 to 2
			Materials Reuse	1 to 2
Materials and Resources, Continued				
Y	2	4	Recycled Content	1 to 2
			Regional Materials	1 to 2
			Rapidly Renewable Materials	1
			Certified Wood	1
9	6	6	Indoor Environmental Quality	15
Y			Prereq 1 Minimum Indoor Air Quality Performance	
			Prereq 2 Environmental Tobacco Smoke (ETS) Control	
			Credit 1 Outdoor Air Delivery Monitoring	1
			Credit 2 Increased Ventilation	1
			Credit 3.1 Construction IAQ Management Plan—During Construction	1
			Credit 3.2 Construction IAQ Management Plan—Before Occupancy	1
			Credit 4.1 Low-Emitting Materials—Adhesives and Sealants	1
			Credit 4.2 Low-Emitting Materials—Paints and Coatings	1
			Credit 4.3 Low-Emitting Materials—Flooring Systems	1
			Credit 4.4 Low-Emitting Materials—Composite Wood and Agrifiber Products	1
			Credit 5 Indoor Chemical and Pollutant Source Control	1
			Credit 6.1 Controllability of Systems—Lighting	1
			Credit 6.2 Controllability of Systems—Thermal Comfort	1
			Credit 7.1 Thermal Comfort—Design	1
			Credit 7.2 Thermal Comfort—Verification	1
			Credit 8.1 Daylight and Views—Daylight	1
			Credit 8.2 Daylight and Views—Views	1
2	1	3	Innovation and Design Process	6
Y			Credit 1.1 Innovation in Design: Specific Title	1
			Credit 1.2 Innovation in Design: Specific Title	1
			Credit 1.3 Innovation in Design: Specific Title	1
			Credit 1.4 Innovation in Design: Specific Title	1
			Credit 1.5 Innovation in Design: Specific Title	1
			Credit 2 LEED Accredited Professional	1
4	1	4	Regional Priority Credits	4
Y			Credit 1.1 Regional Priority: Specific Credit	1
			Credit 1.2 Regional Priority: Specific Credit	1
			Credit 1.3 Regional Priority: Specific Credit	1
			Credit 1.4 Regional Priority: Specific Credit	1
51	7	50	Total	110

STATEMENT ADDRESSING THE REQUIRED FINDINGS FOR A SUBDIVISION, IF APPLICABLE

- 1. The subdivision is in conformance with these land subdivision regulations, the applicable regulations of the zoning ordinance and policies of the comprehensive plan.**

The subdivision is in conformance with the applicable zoning and land subdivision regulations, including the design requirements of Chapter 598, Article III, of the subdivision ordinance. As discussed in the proposed findings for the planned unit development, the project is consistent with the policies of comprehensive plan and the small area plan for this area.

- 2. The subdivision will not be injurious to the use and enjoyment of other property in the immediate vicinity, nor be detrimental to present and potential surrounding land uses, nor add substantially to congestion in the public streets.**

The subdivision will not be injurious to the use and enjoyment of other property in the vicinity, be detrimental to surrounding land uses, or increase congestion in the public streets. The subdivision will combine multiple existing lots into registered land survey tracts suitable for this planned unit development.

- 3. All land intended for building sites can be used safely without endangering the residents or users of the subdivision or the surrounding area because of flooding, erosion, high water table, soil conditions, improper drainage, steep slopes, rock formations, utility easements or other hazard.**

All land intended for building sites can be used safely without endangering the residents or users of the subdivision or the surrounding area because of flooding, erosion, high water table, soil conditions, improper drainage, steep slopes, rock formations, utility easements or other hazards.

- 4. The lot arrangement is such that there will be no foreseeable difficulties, for reasons of topography or other conditions, in securing building permits and in providing driveway access to buildings on such lots from an approved street. Each lot created through subdivision is suitable in its natural state for the proposed use with minimal alteration.**

The lot arrangement will pose no difficulties in securing building permits or in providing driveway access to the site. The proposed lots are suitable in their existing state for the proposed development with minimal alteration.

- 5. The subdivision makes adequate provision for stormwater runoff, and temporary and permanent erosion control in accordance with the rules,**

regulations and standards of the city engineer and the requirements of these land subdivision regulations. To the extent practicable, the amount of stormwater runoff from the site after development will not exceed the amount occurring prior to development.

The City of Minneapolis requires that all new developments meet the stormwater management requirements of Chapter 54 of the City Code of Ordinances in order to be approved for construction. These requirements consist of a reduction in the runoff rate for the 2-year, 10-year, and 100-year, 24 hour storm events, and the removal of 70% of total suspended solids which occur during the 1-1/4" rainfall event.

**Department of Public Works
Engineering Design Division/Right-of-Way Section
309 2nd Avenue South, Room 200
673-2428**

Date: February 11, 2016

To: Hilary Dvorak – Planning
Joel Fussy, City Attorney

From: Robert Boblett, Right of Way

Subject: **Kraus Anderson Block RLS**

I have reviewed the RLS, and we have no right of way issues.

From: [Fussy, Joel M.](#)
To: [Dvorak, Hilary A.](#)
Subject: RE: Kraus Anderson RLS for
Date: Thursday, February 11, 2016 10:06:40 AM

Approved.

Thanks,

Joel Fussy
Assistant Minneapolis City Attorney
(612) 673-2939

From: Dvorak, Hilary A.
Sent: Tuesday, February 09, 2016 2:43 PM
To: Boblett, Robert; Fussy, Joel M.
Subject: Kraus Anderson RLS for

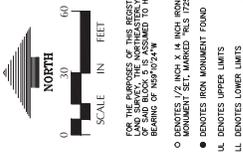
Bob, Joel,

Please review the RLS for the Kraus Anderson Block Redevelopment. Let me know if you have any questions or concerns.

Thanks, Hilary

Hilary Dvorak | Principal City Planner | City of Minneapolis | CPED – Land Use, Design and Preservation
250 South 4th Street | Room 300 | Minneapolis, MN 55415
Phone: 612-673-2639 | Fax: 612-673-2526 | hilary.dvorak@minneapolismn.gov
The City's website is now: www.minneapolismn.gov

REGISTERED LAND SURVEY NO. _____ With Site Plan Overlay



DRAFT

I, **Henry D. Nelson**, Professional Land Surveyor, do hereby certify that, in accordance with the provisions of Minnesota Statutes, Section 508.47, I have surveyed the following described property situated in the County of Hennepin, State of Minnesota:

Lot 1, Block 5 of Mattions Third Addition to Minneapolis, together with the NE 1/2 of the vacated alley adjacent thereto which lies between the Southwesterly extensions of the side lot lines of said Lot 1.

AND

Lot 2, Block 5, "Mattions Third Addition To Minneapolis", together with that portion of the vacated alley in said Block lying Northwesterly of the center line of said alley and between extensions Southwesterly of the Northwesterly and Southwesterly side lines of said Lot 2.

AND

Lots 3, 4, 5 and 6, Block 5, "Mattions Third Addition To Minneapolis", but not including any part of the vacated alley in said Block 5.

AND

Lot 7, 8, 9, 11 and 12, Block 5, "Mattions Third Addition To Minneapolis"

All of the vacated alley in Block 5, "Mattions Third Addition To Minneapolis", except the 3 following described parts:

The Northwesterly one-half of the vacated alley adjacent to Lot 1, said Block, which lies between the Southwesterly extensions of the side lot lines of said Lot 1.

That portion of the vacated alley in said Block lying Northwesterly of the center line of said alley and between the extensions Southwesterly of the Northwesterly and Southwesterly lines of Lot 2, said Block.

That part of the Northwesterly half of the vacated alley, abutting Lot 10, said Block, which lies between the Southwesterly extensions of the Northwesterly line and of the Southwesterly line of said Lot 10.

AND

Lot 10, Block 5, Mattions Third Addition to Minneapolis.

That part of the vacated alley dedicated in Block 5 of the plat of Mattions Third Addition to Minneapolis, which line northwesterly of the centerline thereof, and between the southeasterly extensions of the northwesterly and southwesterly lines of Lot 10, said block and addition.

I hereby certify that this REGISTERED LAND SURVEY was prepared by me or under my direct supervision, that I am a duly Licensed Land Surveyor under the laws of the State of Minnesota and that this REGISTERED LAND SURVEY is a correct representation of said parcel and land.

Dated this _____ day of _____, 201__.

Henry D. Nelson, Professional Land Surveyor
Minnesota License No. 17255

RESIDENT AND REAL ESTATE SERVICES, Hennepin County, Minnesota

I hereby certify that these copies, in 201__ and prior years have been paid for land described on this plat, dated this _____ day of _____, 201__.

Mark V. Chapin, Hennepin County Auditor

By _____ Deputy

SURVEY DIVISION, Hennepin County, Minnesota

Pursuant to MINN. STAT. Sec. 383B.545 (1999) this REGISTERED LAND SURVEY has been approved this _____ day of _____, 201__.

Chris F. Mark, Hennepin County Surveyor

By _____

REGISTRAR OF TITLES, Hennepin County, Minnesota

I hereby certify that this REGISTERED LAND SURVEY NO. _____ was filed in this office this _____ day of _____, 201__ at _____ o'clock _____ m.

Maria McConeck, Registrar of Titles

By _____ Deputy

Notary Public, _____ County, Minnesota
My Commission Expires January 21, 2020

PLANNING COMMISSION, MINNEAPOLIS, MINNESOTA

I, the Secretary of the Planning Commission of the City of Minneapolis, Minnesota, do hereby certify that this REGISTERED LAND SURVEY, and duly authorized such action of the Commission by its Secretary. The prescribed provided by Title 22, Section 598.320 of the Minnesota Code of Ordinances

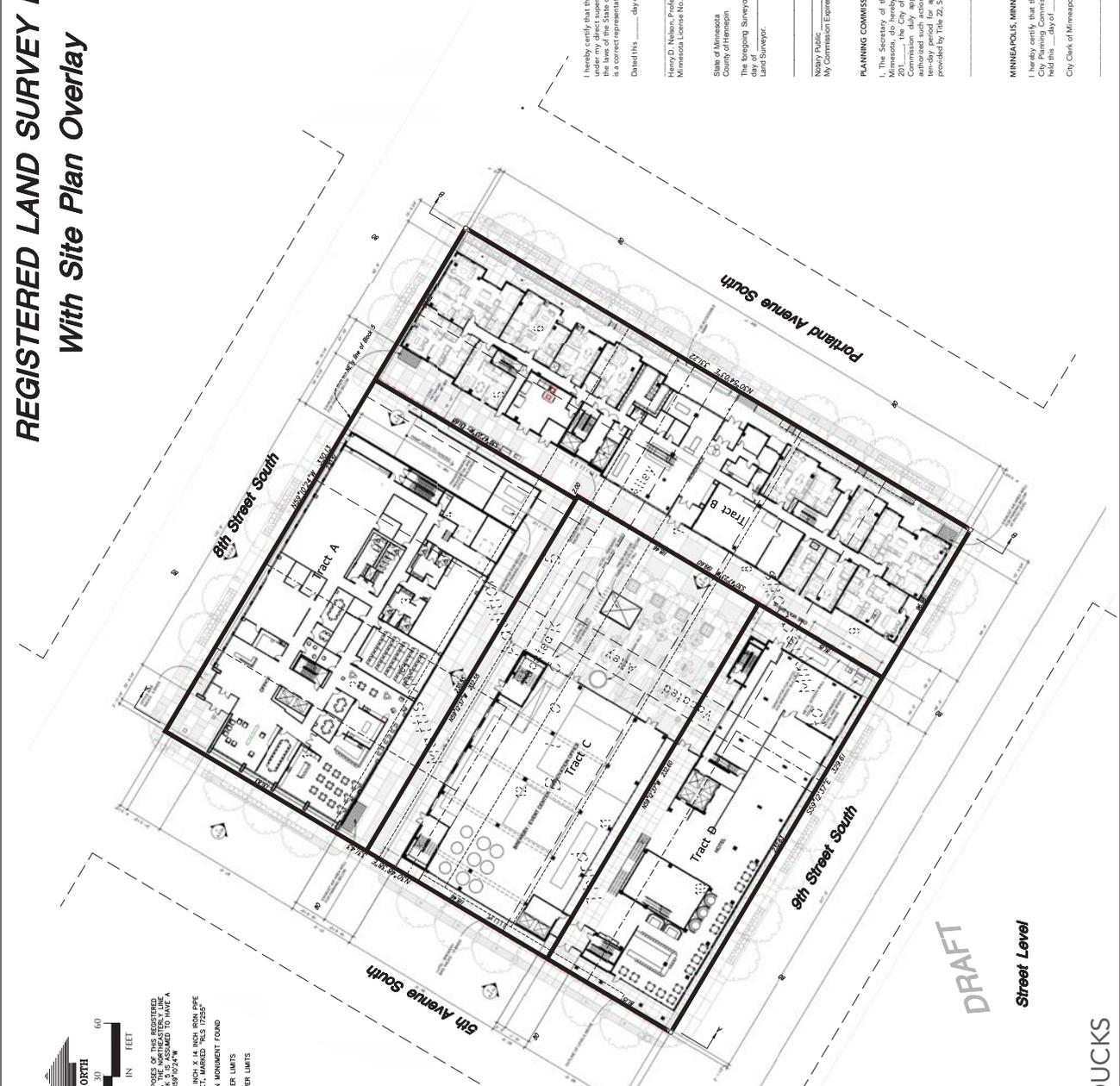
_____, Secretary of Planning Commission

MINNEAPOLIS, MINNESOTA

I hereby certify that this REGISTERED LAND SURVEY was approved by the Planning Commission of the City of Minneapolis at a meeting thereof held this _____ day of _____, 201__.

City Clerk of Minneapolis, Minnesota

_____, Assistant City Clerk



DRAFT

Street Level

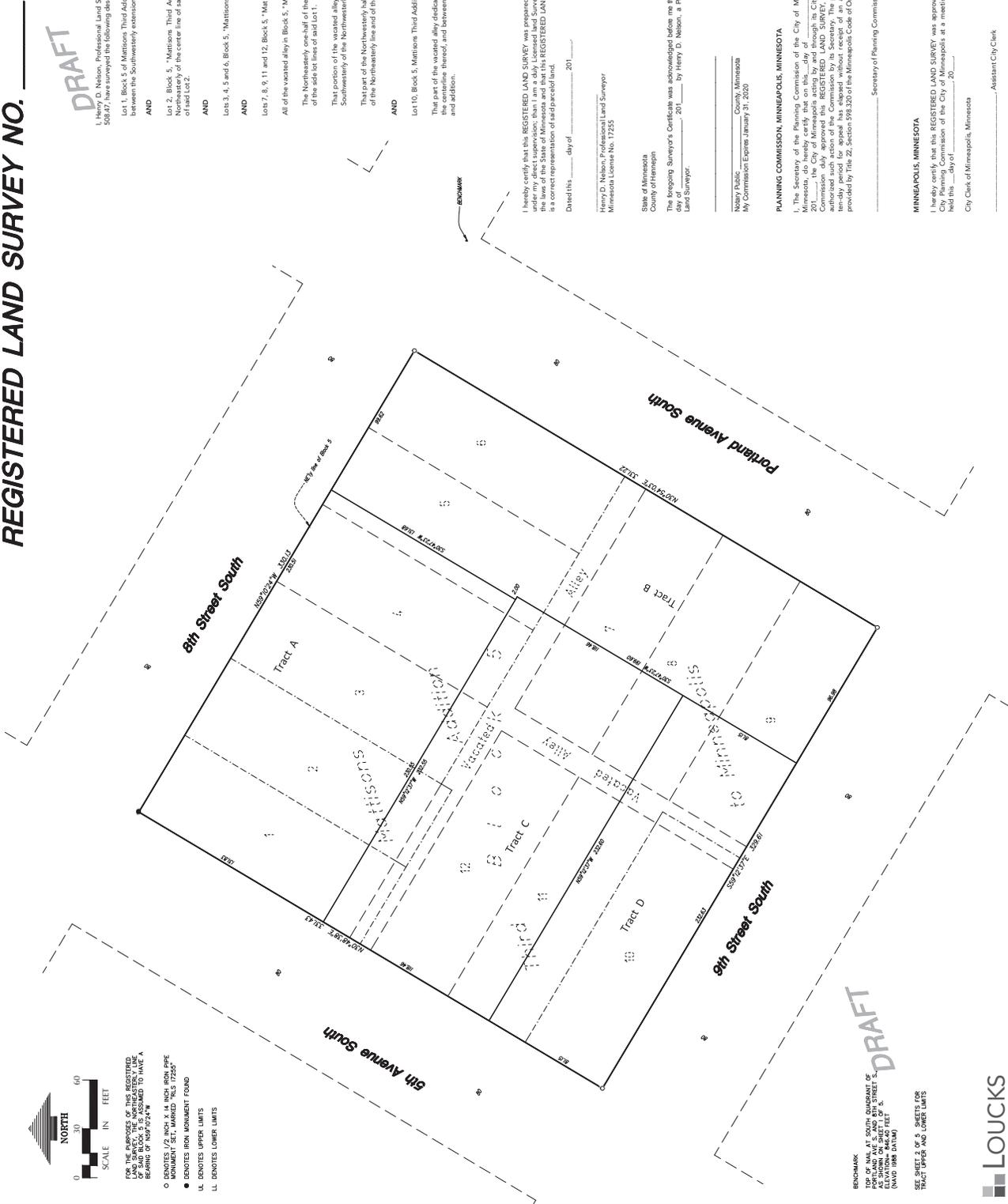


REGISTERED LAND SURVEY NO. _____

DRAFT



- FOR THE PURPOSES OF THIS REGISTERED LAND SURVEY, THE FOLLOWING ASSUMPTIONS HAVE BEEN MADE:
- DENOTES 1/2 INCH X 1/4 INCH IRON PIPE
 - DENOTES IRON MONUMENT FOUND
 - UL DENOTES UPPER LIMITS
 - LL DENOTES LOWER LIMITS



I, Henry D. Nelson, Professional Land Surveyor, do hereby certify that, in accordance with the provisions of Minnesota Statutes, Section 508.07, I have surveyed the following described property situated in the County of Hennepin, State of Minnesota:

Lot 1, Block 5 of Mattions Third Addition To Minneapolis, together with the NE 1/2 of the vacated alley adjacent thereto which lies between the Southwesterly extensions of the side lot lines of said Lot 1.

AND

Lot 2, Block 5, "Mattions Third Addition To Minneapolis", together with that portion of the vacated alley in said Block lying Northwesterly of the center line of said alley and between extensions Southwesterly of the Northwesterly and Southwesterly side lines of said Lot 2.

AND

Lots 3, 4, 5 and 6, Block 5, "Mattions Third Addition To Minneapolis", but not including any part of the vacated alley in said Block 5.

AND

Lot 7, 8, 9, 11 and 12, Block 5, "Mattions Third Addition To Minneapolis"

All of the vacated alley in Block 5, "Mattions Third Addition To Minneapolis", except the 3 following described parts:

The Northwesterly one-half of the vacated alley adjacent to Lot 1, and Block, which lies between the Southwesterly extensions of the side lot lines of said Lot 1.

That portion of the vacated alley lying Northwesterly of the center line of said alley and between the extensions Southwesterly of the Northwesterly and Southwesterly lines of Lot 2, said Block.

That part of the Northwesterly half of the vacated alley, which is between the Southwesterly extensions of the Northwesterly line and of the Southwesterly line of said Lot 1.

AND

Lot 10, Block 5, Mattions Third Addition To Minneapolis.

That part of the vacated alley dedicated in Block 5 of the plat of Mattions Third Addition To Minneapolis, which lies Northwesterly of the center line thereof, and between the southeasterly extensions of the northwesterly and southwesterly lines of Lot 10, said Block and addition.

I hereby certify that this REGISTERED LAND SURVEY was prepared by me or under my direct supervision, that I am a duly Licensed Land Surveyor under the laws of the State of Minnesota and that this REGISTERED LAND SURVEY is a correct representation of said parcel of land.

Dated this _____ day of _____, 201____.

Henry D. Nelson, Professional Land Surveyor
Minnesota License No. 17255

State of Minnesota
County of Hennepin
The foregoing Surveyor's Certificate was acknowledged before me this _____ day of _____, 201____, by Henry D. Nelson, a Professional Land Surveyor.

Natany Public
County, Minnesota
My Commission Expires January 31, 2020

PLANNING COMMISSION, MINNEAPOLIS, MINNESOTA
I, the Secretary of the Planning Commission of the City of Minneapolis, Minnesota, do hereby certify that this REGISTERED LAND SURVEY, and duly authorized such action of the Commission by its Secretary, the prescribed provided by Title 22, Section 598.320 of the Minnesota Code of Ordinances

Secretary of Planning Commission

City Clerk of Minneapolis, Minnesota

Assistant City Clerk

RESIDENT AND REAL ESTATE SERVICES, Hennepin County, Minnesota
I hereby certify that these plat is available in 201____ and prior years there been paid for land described on this plat, dated this _____ day of _____, 201____.

Mark V. Chapin, Hennepin County Auditor

By _____ Deputy

SURVEY DIVISION, Hennepin County, Minnesota
Pursuant to MINN. STAT. Sec. 383B.545 (1999) this REGISTERED LAND SURVEY has been approved this _____ day of _____, 201____.

Chris F. Mark, Hennepin County Surveyor

By _____ Deputy

REGISTRAR OF TITLES, Hennepin County, Minnesota
I hereby certify that this REGISTERED LAND SURVEY NO. _____ was filed in this office this _____ day of _____, 201____, at _____ o'clock _____m.

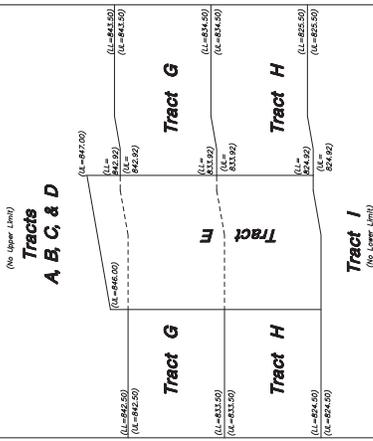
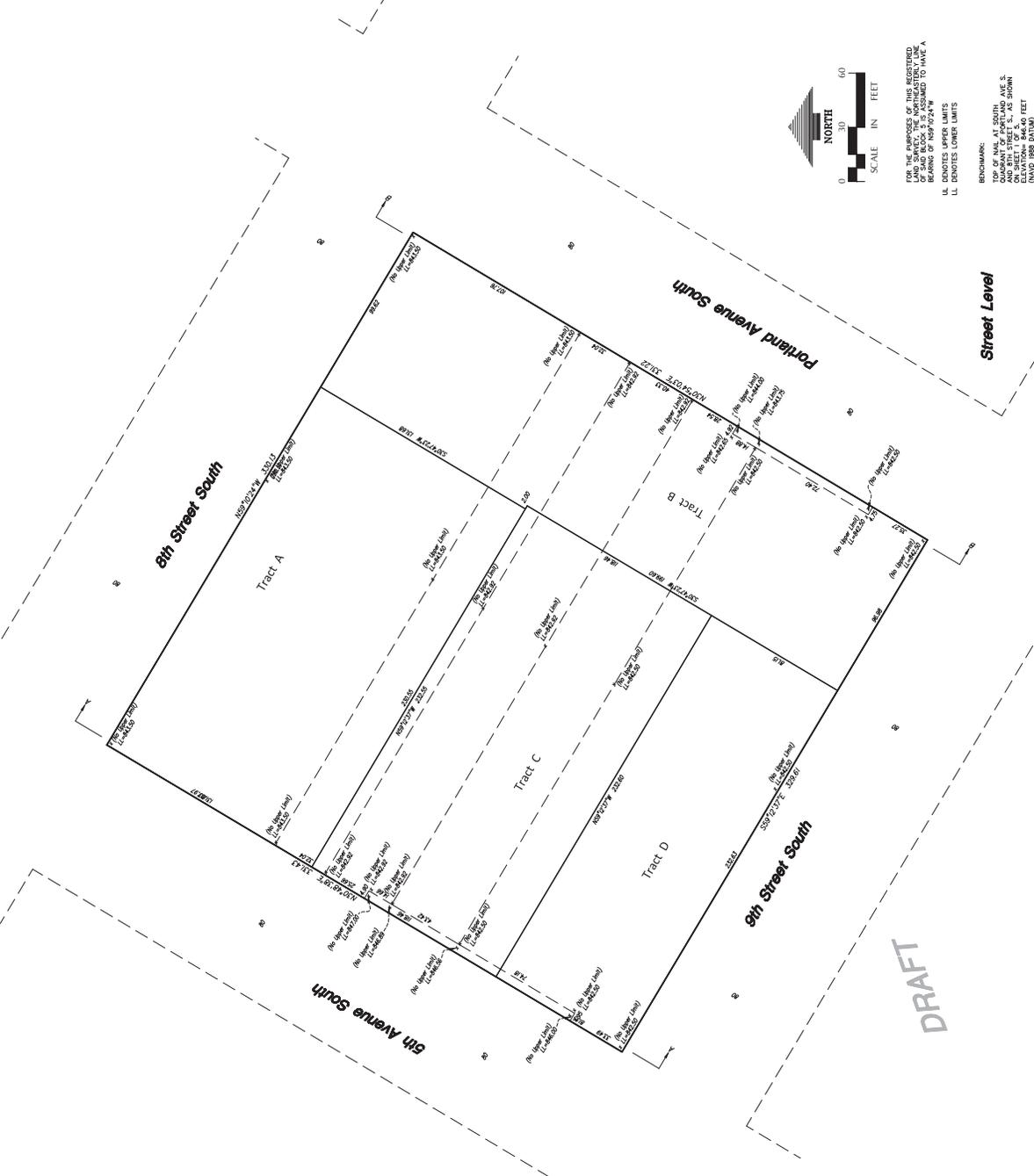
Marisa McCormick, Registrar of Titles

By _____ Deputy



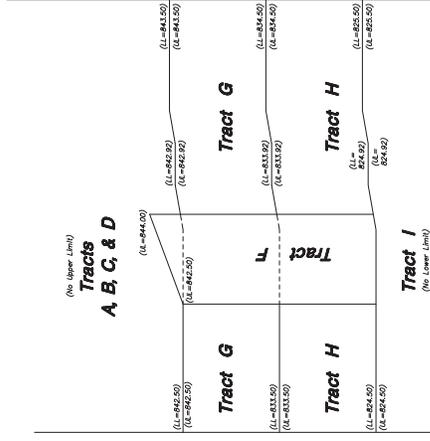
REGISTERED LAND SURVEY NO. _____

DRAFT



Profile View Section "A-A"

(No Upper Limit)
Tracts
A, B, C, & D



Profile View Section "B-B"

(No Upper Limit)
Tracts
A, B, C, & D

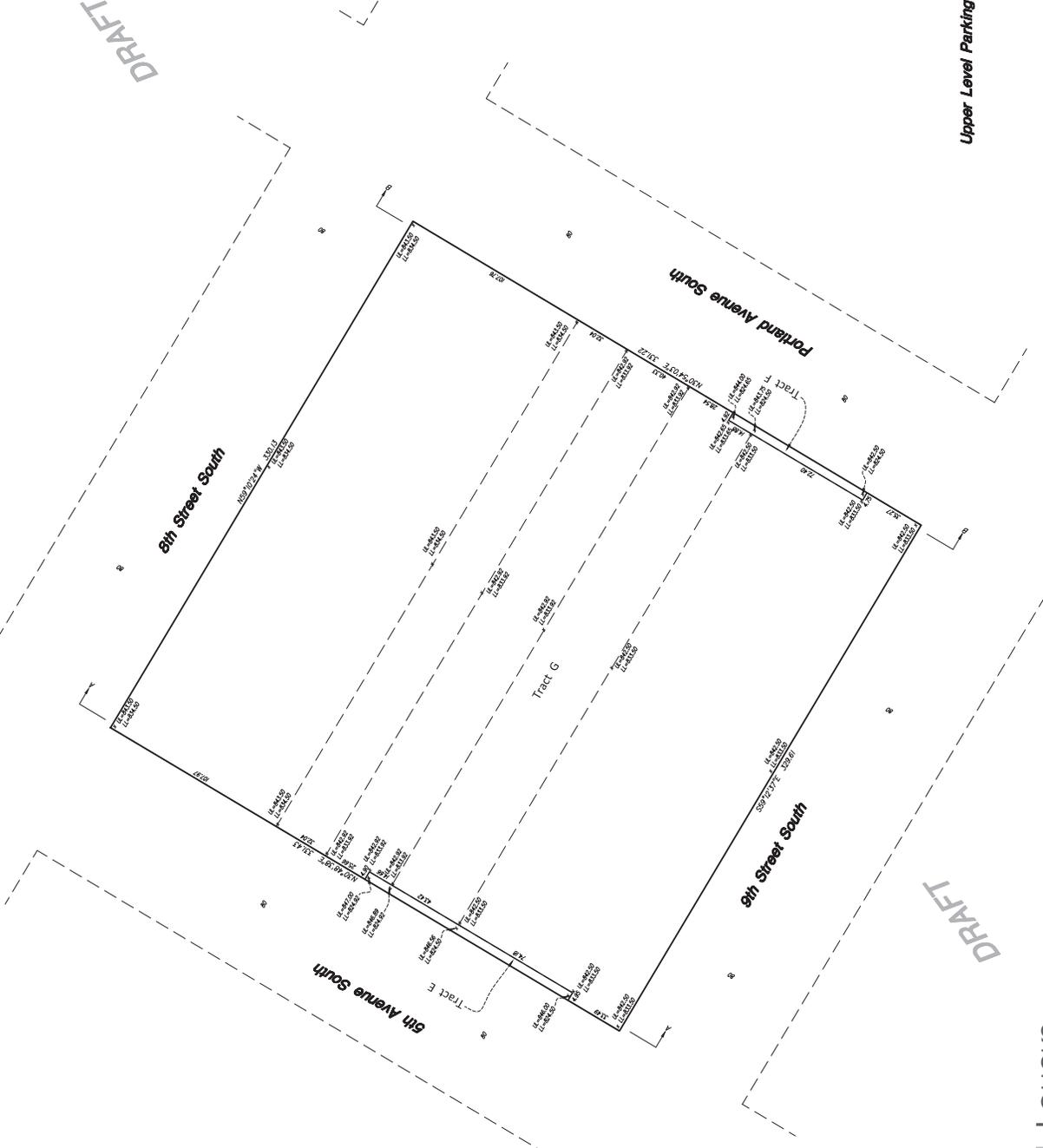


FOR THE PURPOSES OF THIS REGISTERED SURVEY, THE BENCHMARK OF THE BENCHMARK OF NSP/924 W BEARING OF NSP/924 W

- LL DENOTES LOWER LIMITS
- UU DENOTES UPPER LIMITS
- BENCHMARK: TOP OF NAIL AT SOUTH CORNER OF 5TH STREET S.W. AND 8TH STREET S.W. AND INTERSECTION OF 5TH STREET S.W. AND 8TH STREET S.W. (ELEVATION: 846.90 FEET (NAVD 1988 DATUM))

REGISTERED LAND SURVEY NO. _____

DRAFT



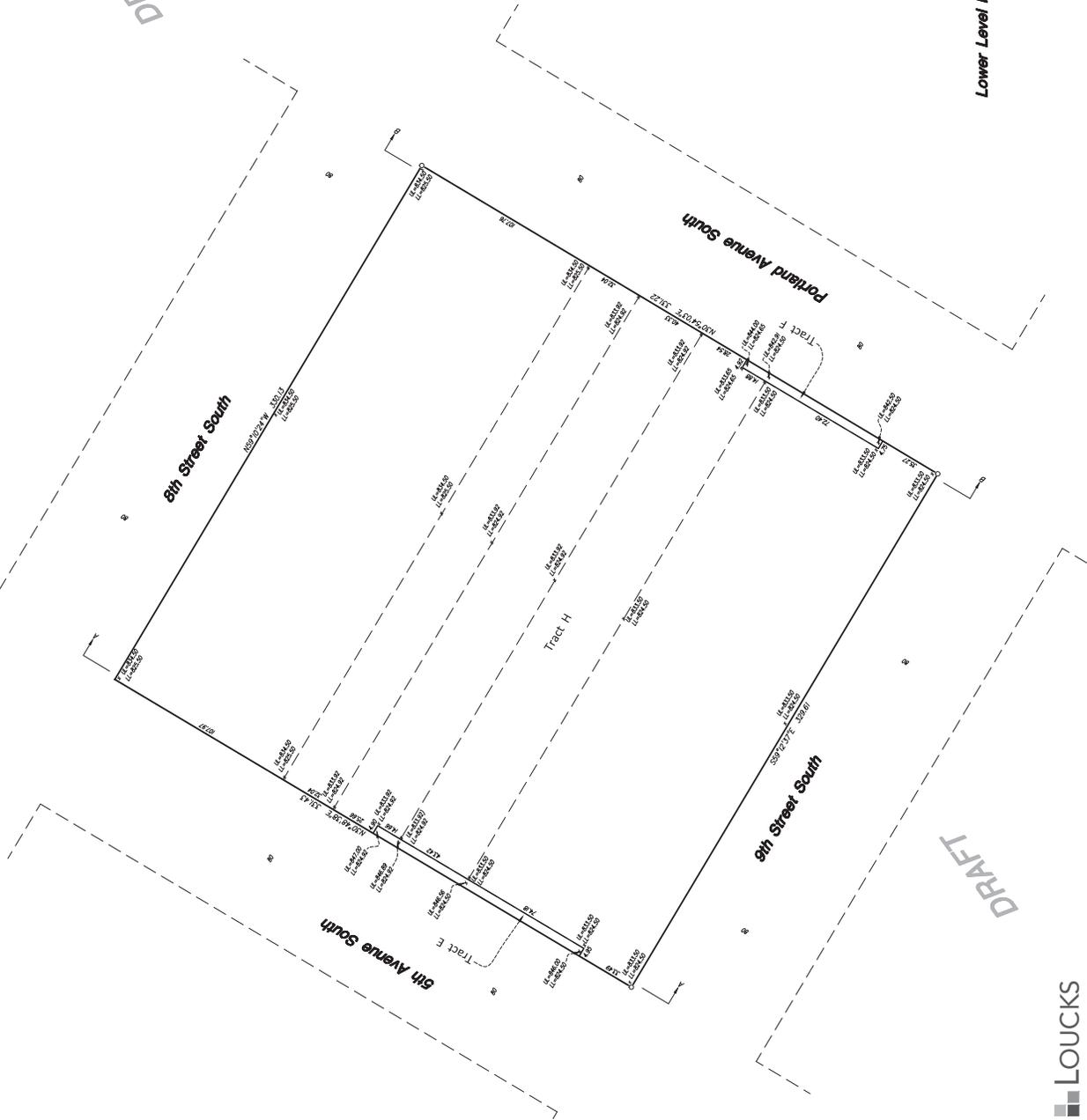
REMARKS:
TOP OF N.A. 1ST SOUTH
CORNER OF PORTLAND AVE. S
ON SHEET 1 OF 5, IS SHOWN
(NAVD 1988 DATUM)

FOR THE PURPOSES OF THIS REGISTERED
LAND SURVEY, THE INTERSECTION LINE
BEARING OF N89°02'24.7\"/>



REGISTERED LAND SURVEY NO. _____

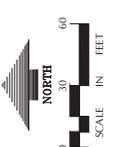
DRAFT



FOR THE PURPOSES OF THIS REGISTERED LAND SURVEY, THE INSTRUMENT TO WHICH THIS BEARING OF N109°02'24\"/>

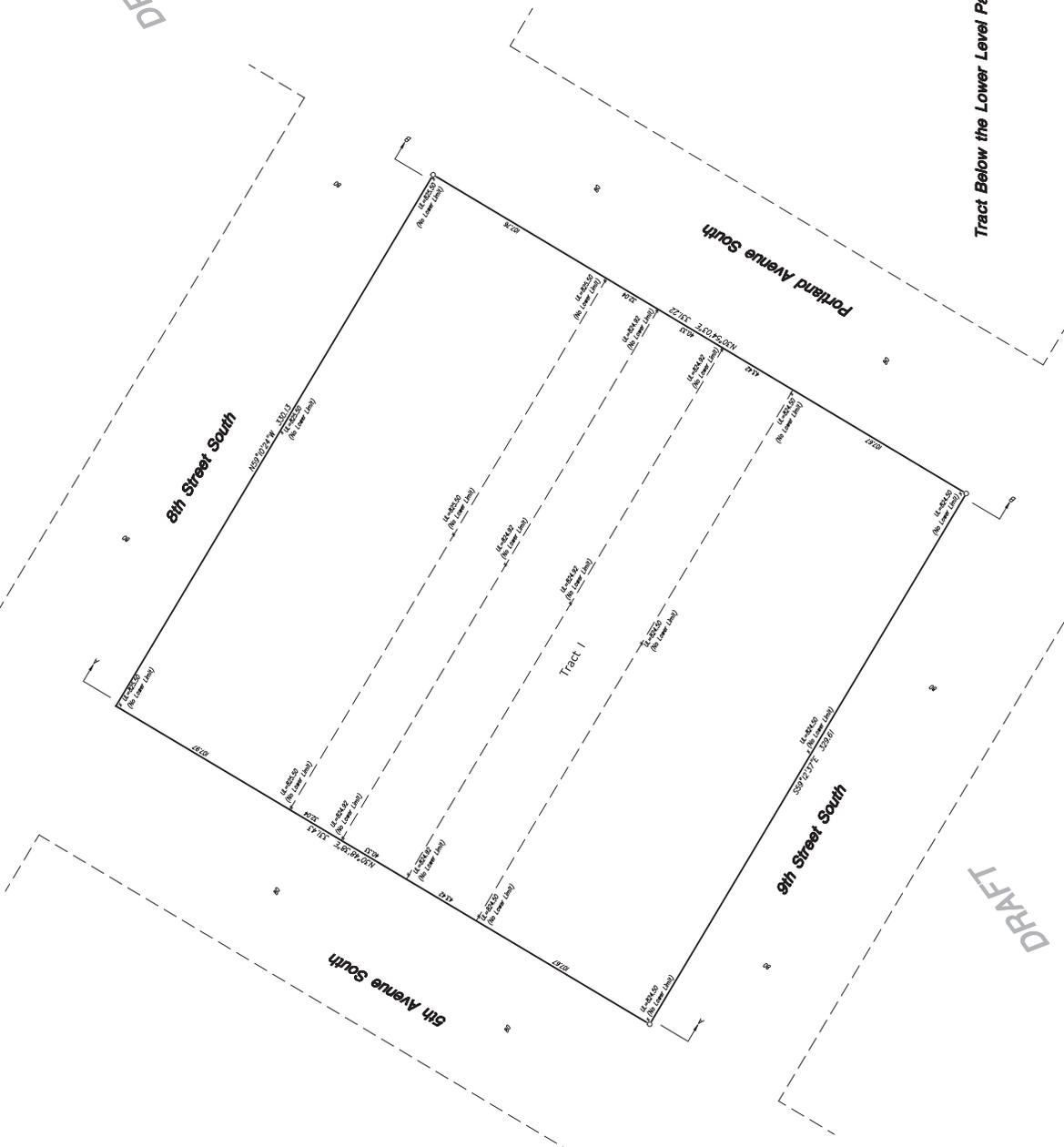
BENCHMARK:
TOP OF NAIL AT SOUTH
CORNER OF PORTLAND AVE. S.
ON SHEET 4 OF 5 OF SURVEY
(NAVD 1988 DATUM)

Lower Level Parking



REGISTERED LAND SURVEY NO. _____

DRAFT



DRAFT

Tract Below the Lower Level Parking

BENCHMARK:
 TOP OF NAIL AT SOUTH
 END OF PORTLAND AVE. S.
 ON SHEET OF S. 10.000
 (NAVD 1988 DATUM)

FOR THE PURPOSES OF THIS REGISTERED
 LAND SURVEY, THE INSTRUMENT TO MAKE A
 BEARING OF N109°02'24\"/>



Travel Demand Management Plan for

Kraus-Anderson Block Redevelopment

Prepared for:

Kraus-Anderson, Incorporated
525 South 8th Street
Minneapolis, MN 55404
612.332.7281
www.krausanderson.com

Prepared by:

Westwood Professional Services
7699 Anagram Drive
Eden Prairie, MN 55344
(952) 937-5150
www.westwoodps.com

Project Number: R0007118.00

Date: 2/22/2016

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Appendix A: Traffic Impact Study

1.0 -- INTRODUCTION

The Vision and Purpose of the proposed Kraus-Anderson Block Redevelopment is to transform an entire city block, consisting of a surface parking lot and antiquated office building into a vibrant, high-density mixed use destination in the Elliot Park neighborhood of downtown Minneapolis consistent with several key sources of guidance. This particular area of the city is currently characterized by several large surface parking lots



and is in need of creative infill redevelopment. The proposed project will serve as a catalyst for future improvements and investment in this area of downtown Minneapolis.

1.1 -- MIXED USE PROGRAM SUMMARY

The program for this redevelopment includes three distinct and active uses:

- A new Kraus-Anderson (KA) Corporate Home Office of approximately 95,000 square feet. KA has been located on this site for over 75 years.
- A new Type 1 residential midrise building of approximately 306 dwelling units.
- A new 148-key Finnegan’s House Boutique Hotel/ Micro Brewery/ Innovation Center.

Table 1-1: Land Use Changes with Proposed Development

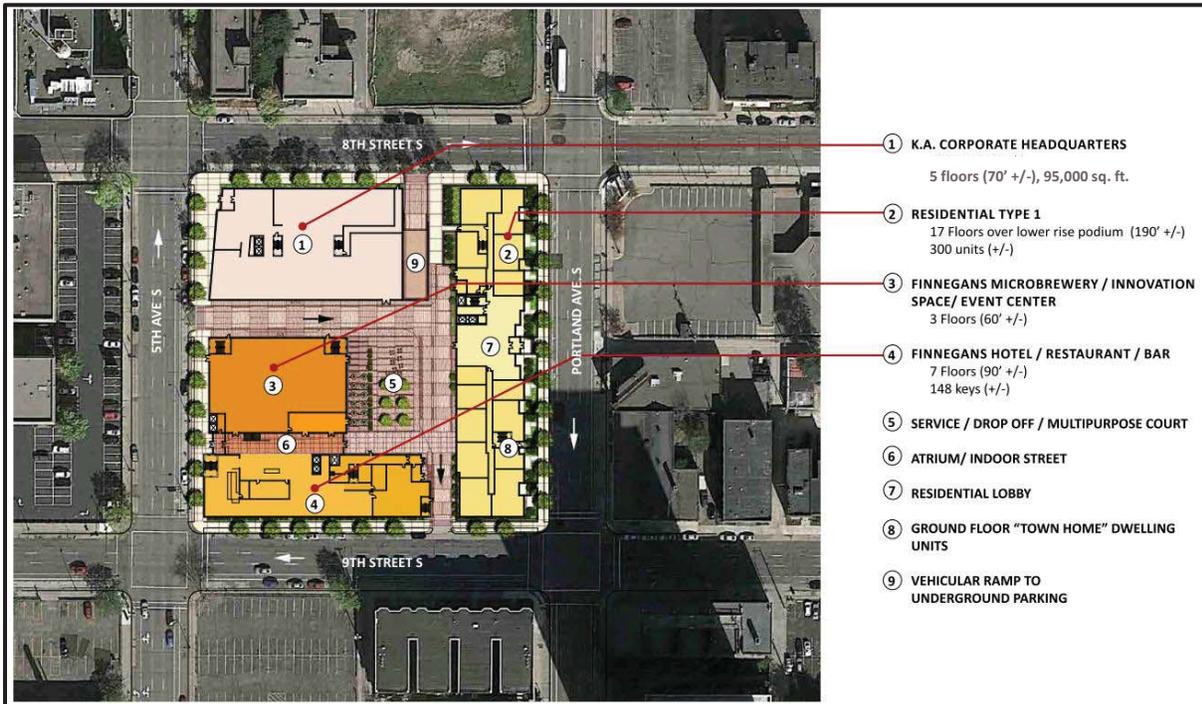
Existing Land Uses	Proposed Land Uses
<u>Office Building</u> (30 ksf.)	<u>Office Headquarters Building</u> (95 ksf)
<u>Off-Street Parking</u> (169 pay-parking stalls + 100 KA employee parking stalls + 13 visitor stalls = 299 total stalls)	<u>Residential</u> – 306 dwelling units (57 studio apts.; 75 alcove apts.; 82 1-BR apts.; 69 2-BR apts.; 4 2-BR+den apts.; 19 3-BR apts.)
	<u>Finnovation Office Space</u> (12ksf)
	<u>“Brewtel” Boutique Hotel</u> (148 rooms; 110,000 sq. ft.)
	<u>Hotel dining/meeting space</u> (8,400sf, 200 seats)
	<u>Brewery</u> (12 ksf)
	<u>Event Center</u> (13ksf, 750 seats)
	<u>Underground Parking</u> (530 stalls)

(Source: Westwood, September, 2015)

Figure 1-1: Site Location (Source: KA Block Mixed Use Redevelopment Design Vision)



Figure 1-2: Concept Layout (Source: KA Block Mixed Use Redevelopment Design Vision)



The development lies completely within the B4N Downtown Neighborhood Zoning District which promotes higher density development surrounding the Downtown office core. The development also lies in the Downtown Parking Overlay District, which according to Article IX of the Minneapolis Municipal Code, states:

“The DP Downtown Parking Overlay District is established to preserve significant and useful buildings and to protect the unique character of the downtown area and the mixed-use downtown neighborhoods by restricting the establishment or expansion of surface parking lots and establishing certain minimum and maximum off-street parking standards in the downtown area.”

The site is surrounded by one-way streets:

- 5th Avenue South is a three-lane arterial northbound
- Portland Avenue is a three-lane arterial southbound
- South 8th Street is a three-lane arterial eastbound
- South 9th Street is a three-lane arterial westbound

The site lies along several Metro Transit lines. Nine different routes providing full, limited, or express bus transit opportunities are located at or nearby this site.

The site is also bike- and pedestrian-oriented, with on-street bike routes along both Fifth Avenue South and Portland Avenue. The streets represent a one-way pair with Fifth Avenue South heading northbound and Portland Avenue heading southbound. Further, the site has sidewalks along all four sides.

Kraus-Anderson will propose TDM strategies similar to those identified in other recent plans in downtown Minneapolis. The added dimensions of the unique mix of uses, the downtown location combined with the numerous adjacent transit and pedestrian facilities will serve to reduce traffic demand to and from this development.

This TDMP will identify the alternative transportation options in the vicinity of the site, will discuss the change in parking and site generated traffic, and will include strategies to encourage the use of these alternative modes.

2.0 -- PEDESTRIAN, BICYCLE AND TRANSIT CONSIDERATIONS

The proposed development's location between South 8th Street and South 9th Street and between 5th Avenue South and Portland Avenue affords the future office and Brewtel employees, apartment residents and hotel visitors with many opportunities for the use of alternative transportation modes.

2.1 -- TRANSIT

There are numerous transit opportunities around this site (see Figure 3).

Bus Stop #49427 – Corner of South 9th Street and Portland Avenue

Served by:

- Local Bus Route 9 – service between Minnetonka/Saint Louis Park and south Minneapolis via downtown Minneapolis.
 - Weekday scheduled stops at this location vary from 15 to 50 minutes between 5:13 a.m. and 1:14 a.m.
 - Saturday service varies from 30 to 60 minutes between 6:06 a.m. and 1:14 a.m.
 - Sunday service varies from 30 to 60 minutes between 6:08 a.m. and 1:23 a.m.

- Local Bus Route 20 – Weekday rush hour service only

Serves riders:

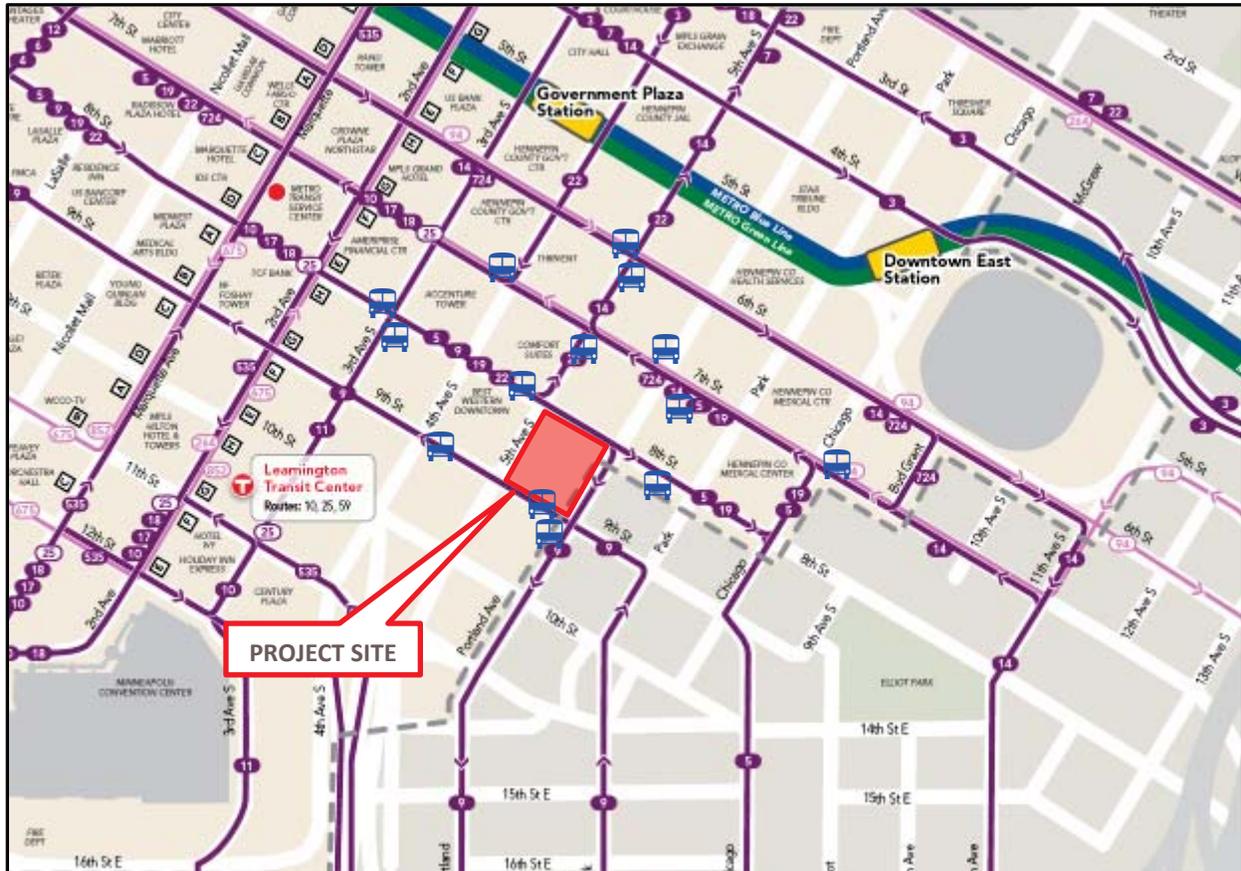
- Southbound from Northstar trains to 5th Avenue and 7th Street every half hour between 6:00 a.m. and 8:33 a.m.
- Northbound to Northstar trains from 9th Street and Chicago Avenue every half hour between 3:34 p.m. and 6:05 p.m.

- Local Bus Route 643 – Weekday rush hour service only

Serves riders:

- Westbound from downtown Minneapolis to Minnetonka every 30 minutes to 60 minutes between 6:02 a.m. and 9:06 a.m.
- Eastbound from Minnetonka to downtown Minneapolis every 30 minutes to 60 minutes between 3:08 p.m. and 6:27 p.m.

Figure 2-1: Transit Routes



Downtown Map Legend

- Downtown Zone Limit**
Ride in the Downtown Zone for 50¢ (transfers not available). Board any bus or train going the direction you want to go.
- Frequent Local Buses**
Serve all stops and operate at least every 30 min. during middays on weekdays, more often during rush hours.
- All-Day Local Buses**
Serve all stops and operate throughout the day but may offer less frequent service.
- All-Day Express Buses**
Limited-stop or non-stop service that operates throughout the day on weekdays but may offer less frequent service.
- METRO Blue Line**
Trains stop at all stations shown.
- Bus Stops in proximity to Project Site**
- METRO Green Line**
Trains stop at all stations shown.
- Northstar Commuter Rail**
Operates primarily during rush hours.
- Rail Platform**
- Transit Center/Station**
- Transit Service Center**
Get passes, route information and other transit help.
- Lettered Stop**
Express buses serve these stops on Marquette and 2nd avenues.
- Free Rides on Nicollet Mall**
Look for buses marked "Free Ride" between the Convention Center and Washington Avenue in Minneapolis.

SOURCE: Metro Transit Website, 2015.

- **Express Bus Route 663** – Weekday rush hour service only

Serves riders:

- Eastbound from Minnetonka to downtown Minneapolis every 30 minutes to 60 minutes between 6:17 a.m. and 9:43 a.m.
- Westbound from downtown Minneapolis to Minnetonka every 30 minutes to 60 minutes between 2:58 p.m. and 6:46 p.m.

Bus Stop #17912 – South 8th Street between Park and Portland Avenues

Served by:

- **Local Bus Route 5** – service between Mall of America and Brooklyn Center via south, downtown and north Minneapolis:
 - Weekday, Saturday and Sunday scheduled stops at this location vary from 3 to 60 minutes twenty-four hours a day.
- **Local Bus Route 19** – service between downtown Minneapolis and Brooklyn Center via north Minneapolis
 - Weekday, Saturday and Sunday scheduled stops at this location vary from 3 to 60 minutes twenty-four hours a day.

Bus Stop #17911 – South 8th Street between 4th and 5th Avenues South

Served by:

- **Local Bus Route 5** – service between Mall of America and Brooklyn Center via south, downtown and north Minneapolis:
 - Weekday, Saturday and Sunday scheduled stops at this location vary from 3 to 60 minutes twenty-four hours a day.
- **Local Bus Route 9** – service between Minnetonka/Saint Louis Park and south Minneapolis via downtown Minneapolis.
 - Weekday scheduled stops at this location vary from 15 to 50 minutes between 5:13 a.m. and 1:14 a.m.
 - Saturday service varies from 30 to 60 minutes between 6:06 a.m. and 1:14 a.m.
 - Sunday service varies from 30 to 60 minutes between 6:08 a.m. and 1:23 a.m.

- Local Bus Route 19 – service between downtown Minneapolis and Brooklyn Center via north Minneapolis
 - Weekday, Saturday and Sunday scheduled stops at this location vary from 3 to 60 minutes twenty-four hours a day.

- Local Bus Route 39 – Weekday rush hour service only
Serves riders:
 - Southbound from downtown Minneapolis to Chicago and 26th Street every 30 minutes between 6:29 a.m. and 7:49 p.m.
 - Northbound from 5th Avenue South and 27th Street to downtown Minneapolis every 30 minutes between 3:37 p.m. and 5:26 p.m.

In addition to bus transit, both Government Plaza Station and Downtown East Station serving METRO BLUE and GREEN Lines of light rail transit are approximately five blocks away. The BLUE Line provides LRT service between Target Field Station and Mall of America Station via MSP International Airport. The GREEN Line provides LRT service between Target Field Station and Union Depot in Saint Paul via the University of Minnesota.

2.2 -- BICYCLE

Downtown Minneapolis is heavily traveled by bicyclists. The following two figures illustrate the significant bicycle opportunities and usage present in downtown Minneapolis.

- As shown on Figure 2-2, there are designated on-street bike routes along 5th Avenue South, Portland Avenue and South 9th Street that tie into the elaborate bike trail system of Minneapolis. This system would enable potential residents to easily travel to other downtown locations such as the Viking Stadium, the central business district and Nicollet Mall, as well as venture to the east across the Mississippi River into Northeast Minneapolis, Dinkytown and the University of Minnesota area.

- As shown on Figure 2-2, there are six NiceRide Minnesota stations within four blocks of the site. NiceRide Minnesota is a non-profit bike sharing program being deployed throughout the Twin Cities, and is an available strategy to reduce auto trips. (NiceRide stations are also shown on Figure 2-4.)

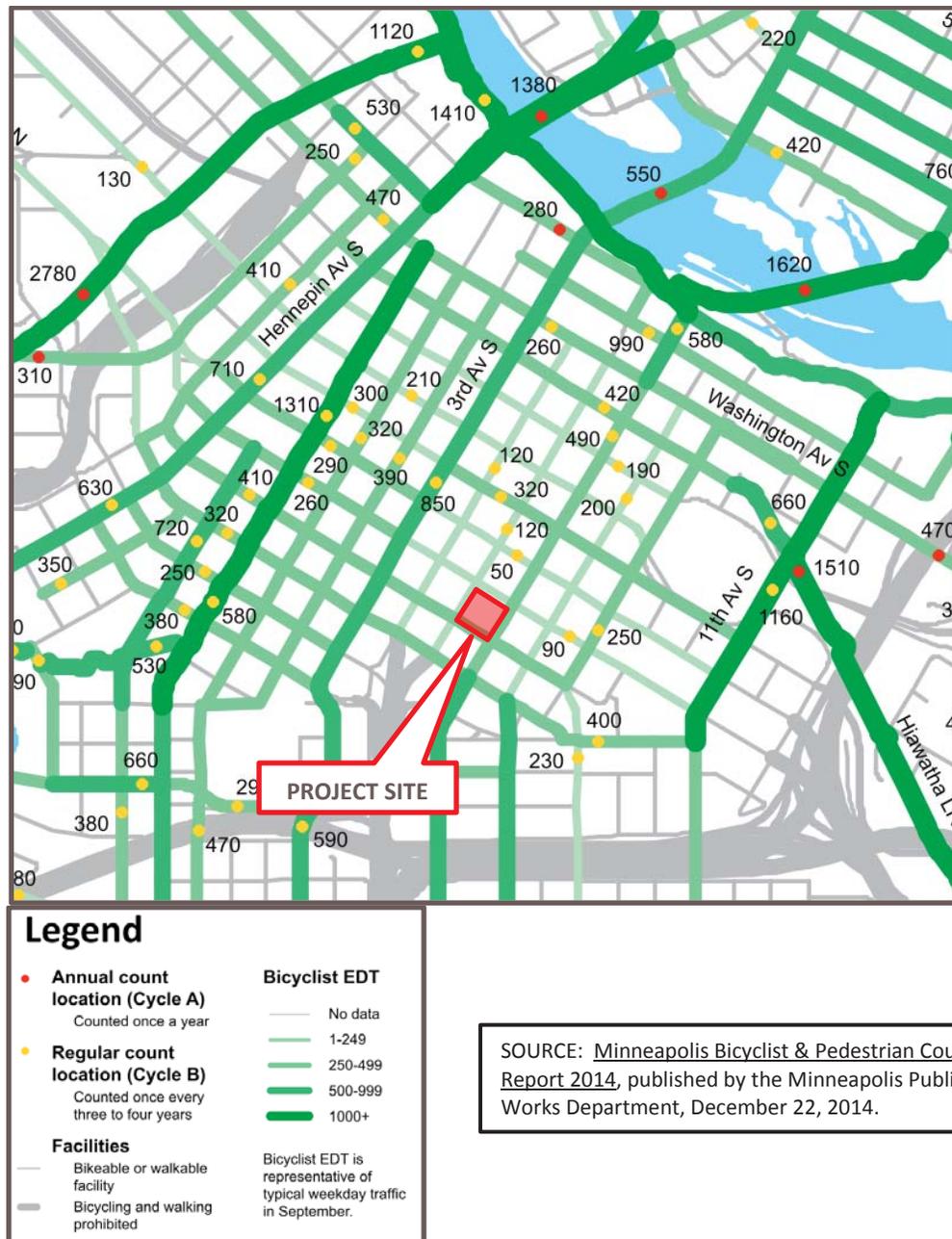
Figure 2-2: Existing Bike Routes



<p>Streets</p> <ul style="list-style-type: none"> Busier streets Local streets One-way traffic Bicycles prohibited or strongly discouraged Railroad tracks Selected bridges 		<p>Bicycle Routes</p> <ul style="list-style-type: none"> Off-street bicycle trails On-street bicycle lanes and shoulders Shared lanes and bicycle boulevards Pedestrian paths and bridges (bicycles allowed) Low-Stress Bicycle Network Trails, bike boulevards, & quieter streets in Minneapolis 		<p>Points of Interest</p> <ul style="list-style-type: none"> Colleges Arts & Entertainment Bike shops with repairs Other bike-related businesses Nice Ride station Light rail Hi-frequency buses Transit hubs
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- Figure 2-3 illustrates the estimated daily bicycle traffic along the streets in downtown Minneapolis, as reported by the Minneapolis Public Works Department.¹ Daily bicycle traffic along the designated bike routes of Portland Avenue is approximately 490 bicyclists per day, while South 8th Street has a daily count of approximately 90 bicyclists per day.

Figure 2-3: Bicyclist Estimated Daily Traffic (EDT)



¹ Minneapolis Bicyclist & Pedestrian Count Report 2014, published by the Minneapolis Public Works Department, December 22, 2014.

2-3 – CAR-SHARING

In recent years, several car-sharing options have become available throughout the Twin Cities, and especially in downtown Minneapolis. Car-sharing companies, such as HOURCAR, Car2Go, ZipCar and Enterprise CarShare make fleets of vehicles available to customers for short-term rentals. These rentals are geared to registered customers who do not own personal vehicles, but require a vehicle for short-term personal use. Gasoline, insurance and maintenance are included in the rental cost.

HOURCAR, Enterprise CarShare and ZipCar offer vehicles in designated parking spaces, while Car2Go offers cars that can be collected and parked on city streets. Figure 2-4 shows the proximity to these stations near the KA Block redevelopment.

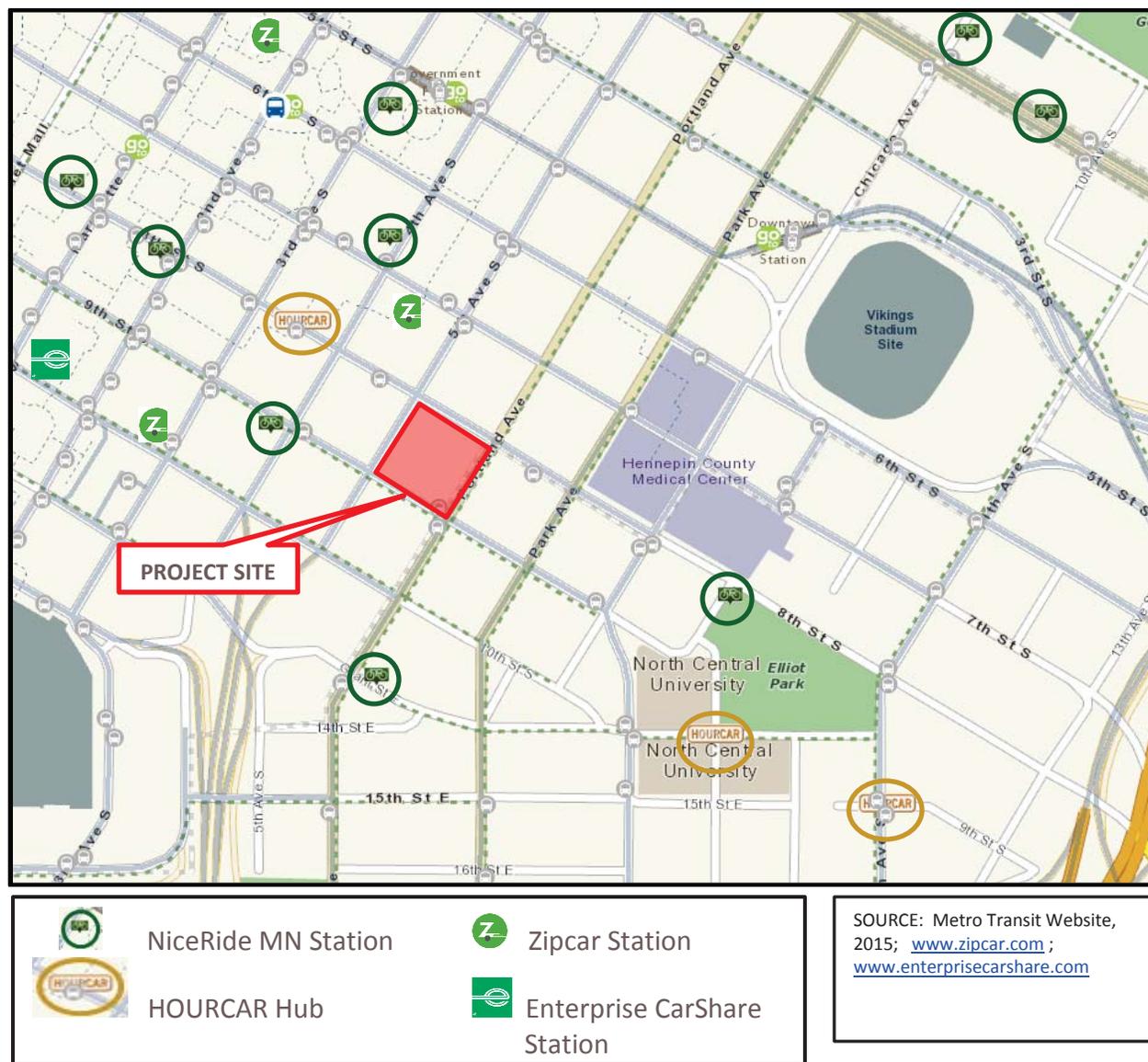
There is a hub for HOURCAR located less than two blocks to the northwest of the site. This would be within walking distance and would be a positive amenity when a personal vehicle is needed on a temporary basis.

Other shared vehicle providers, such as Car2Go (<https://www.car2go.com/en/minneapolis/>), Enterprise CarShare (<https://www.enterprisecarshare.com/us/en/home.html>) and Zipcar (<http://www.zipcar.com/minneapolis/find-cars>) offer vehicles at sites throughout Minneapolis.

Enterprise CarShare (<https://www.enterprisecarshare.com/us/en/home.html>) has two parking stations within downtown Minneapolis – one at the Mar-Ten Ramp and another on Oak Grove Street in the Loring Park neighborhood.

ZipCar has a parking station at the Centre Village Parking Ramp just to the northwest of the K-A Block at 700 5th Ave South. There is also another ZipCar parking station at the surface parking lot at 216 South 10th Street, adjacent to the Ameriprise Financial Center.

Figure 2-4: 2015 Shared Car and NiceRide MN Station Locations

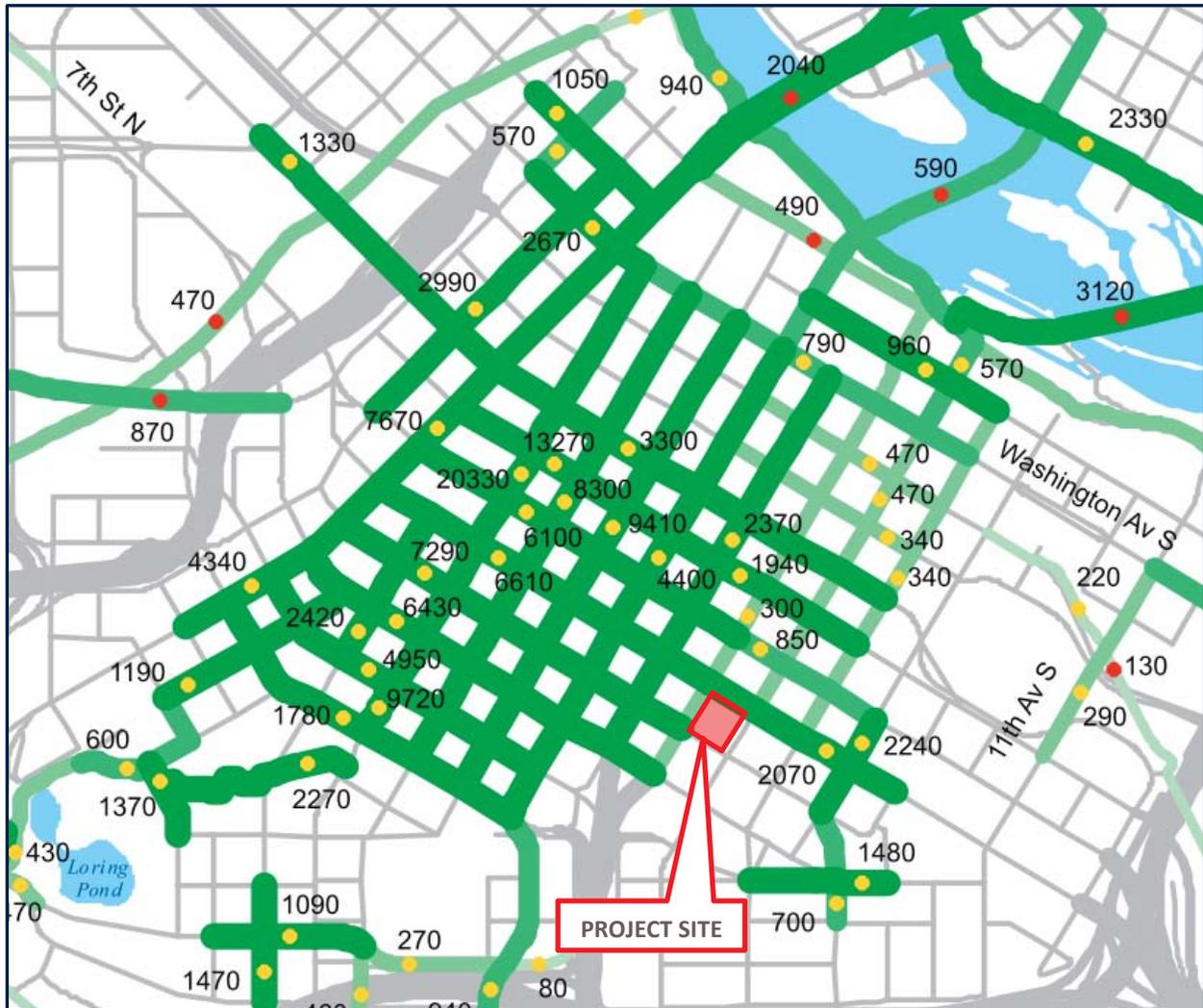


2-4 -- PEDESTRIANS

Pedestrian activity is quite heavy in downtown Minneapolis. Figure 2-5 shows the estimated trips per day by pedestrians in this area, as reported by the Minneapolis Public Works Department.²

² Minneapolis Bicyclist & Pedestrian Count Report 2014, published by the Minneapolis Public Works Department, December 22, 2014.

Figure 2-5: Pedestrian Estimated Daily Traffic (EDT)



Legend

- **Annual count location (Cycle A)**
Counted once a year
- **Regular count location (Cycle B)**
Counted once every three to four years
- Facilities**
 - Bikeable or walkable facility
 - Bicycling and walking prohibited

Pedestrian EDT

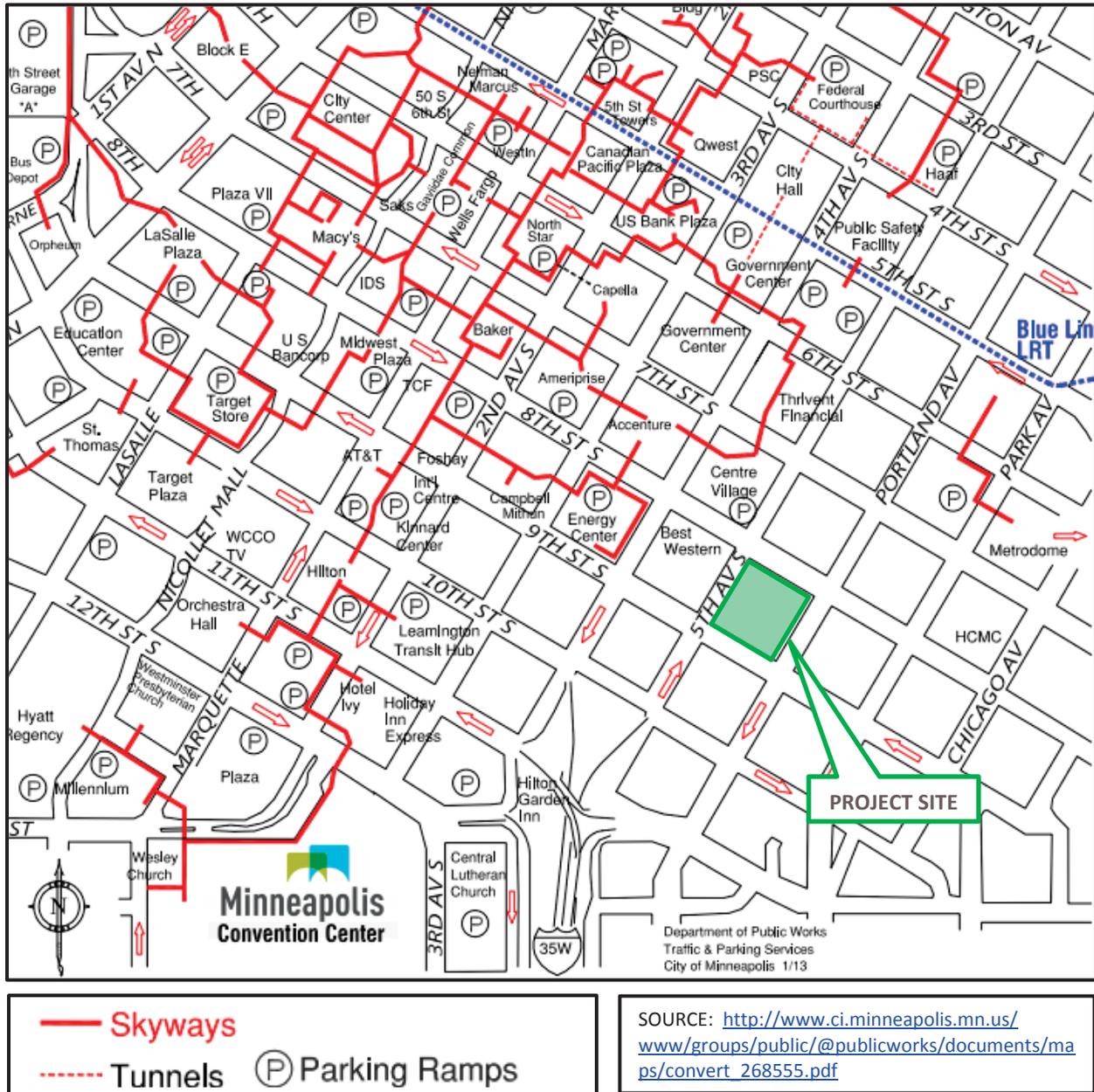
- No data
- 1-249
- 250-499
- 500-999
- 1000+

Pedestrian EDT is representative of typical weekday traffic in September.

SOURCE: Minneapolis Bicyclist & Pedestrian Count Report 2014, published by the Minneapolis Public Works Department, December 22, 2014.

- Sidewalks exist along all public streets that are adjacent to the project site. These sidewalks provide pedestrian access to the robust sidewalk and pedestrian trail network in this area of Minneapolis.
- South 8th Street recorded pedestrian activity varying between 2,000 and 6,600 pedestrians per day.
- Downtown Minneapolis has an eight-mile skyway system that provides climate-controlled pedestrian access between buildings (see Figure 2-6). While no “skyway-ready” connections exist to the KA Redevelopment Block, current connections to the skyway system are one block north of the project site in the Centre Village building.
- In the near future, pedestrian activity will greatly increase in downtown Minneapolis as new development replaces surface parking lots.

Figure 2-6: Downtown Minneapolis Skyway Map

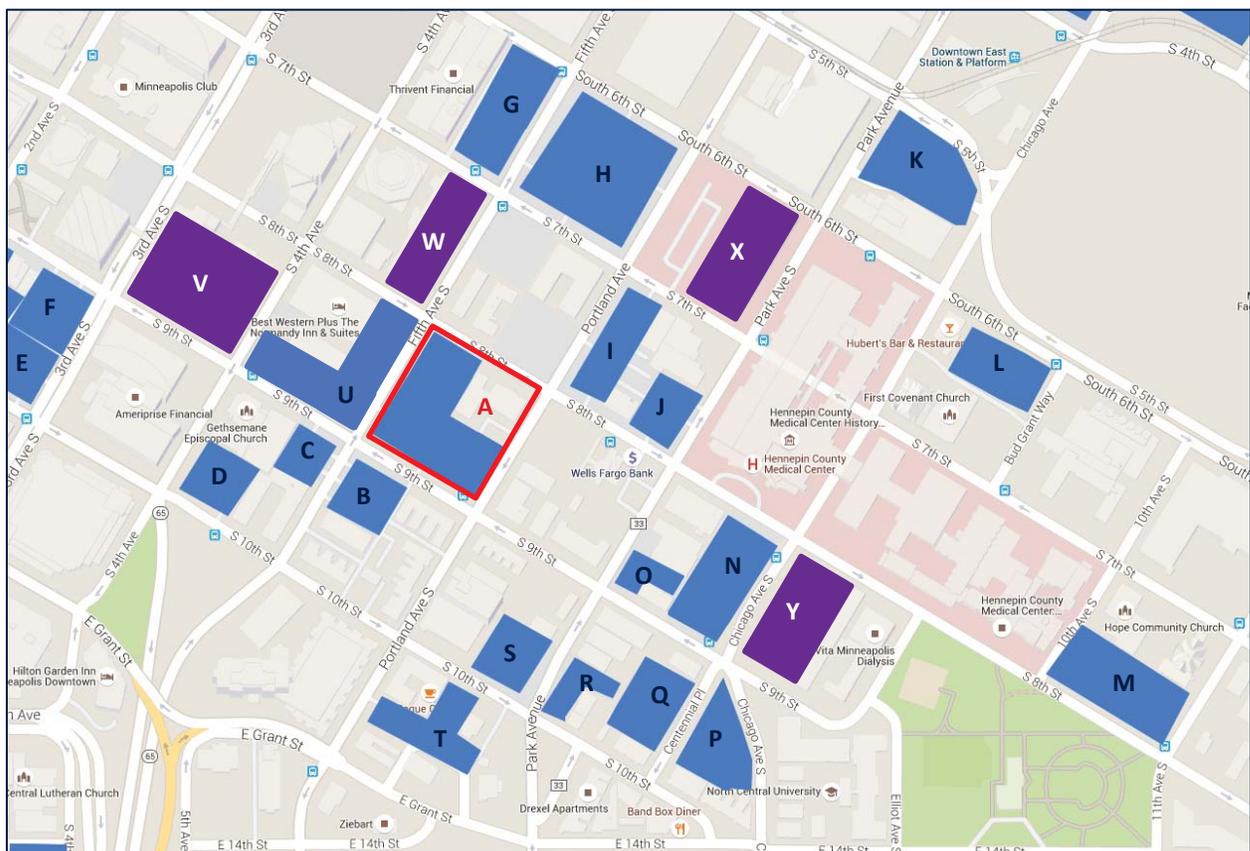


3.0 -- PARKING CONSIDERATIONS

3.1 – DISPLACED PARKING

The KA Block Redevelopment will displace 299 surface parking spaces. There exist several public and private surface lots and parking ramps around the KA Block site (See Figure 3-1). The blue areas represent surface lots, while the purple areas represent multi-level parking ramps

Figure 3-1: Parking Lots and Ramps in Study Area



(Source: Base Map: "Where are downtown's surface parking lots?", [Minneapolis StarTribune](#), April 2, 2014; Data is current as of September, 2015)

An inventory was taken to review parking capacities at these lots and ramps (See Table 3-1). This inventory shows there are over 7,000 public parking stalls available in the immediate study area for drivers whose parking will be displaced once the existing KA surface lot is closed. Note that the colors of the letter codes reflect either the KA block (red), surface lots (blue) or parking ramps (purple).

Table 3-1: Parking Lot and Ramp Capacities in Study Area

Code	Parking Lot or Ramp	Location	Restricted or Public	Parking Capacity
A	Kraus-Anderson Lot	Kraus-Anderson Block	Public	299 stalls
B	Kensington Lot (Monthly)	5 th Ave S. & S. 9 th St.	Restricted	102 stalls
C	Benson Lot #20	5 th Ave S. & S. 9 th St.	Public	56 stalls
D	Benson Parking Service	S. 4 th Ave. & S. 10 th St.	Public	68 stalls
E	Transpark Benson Lots #2 & #14	3 rd Ave S. & S. 10 th St.	Public	253 stalls
F	Benson Lot #13	3 rd Ave S. & S. 10 th St.	Public	67 stalls
G	Thrivent Lot #201	5 th Ave S. & S. 6 th St.	Restricted	103 stalls
H	Thrivent Lot #203	333 S. 7 th Street	Public	630 stalls
I	Portland East	705 Portland Ave	Public	110 stalls
J	Impark Lot #73	620 S. 8 th St.	Public	85 stalls
K	Standard Parking (Monthly)	S. 5 th & Park Ave. S.	Restricted	96 stalls
L	First Covenant Church	S. 6 th St. & Bud Grant Way	Public	207 stalls
M	Allied Parking	S. 8 th St. & 11 th Ave. S.	Public	144 stalls
N	Smith Brothers Lot #13	S. 8 th Street & Chicago Ave S.	Public	166 stalls
O	Smith Brothers Lot #14	S. 9 th Street & Park Ave S.	Public	48 stalls
P	Impark Lot #72	900 Centennial Place	Public	114 stalls
Q	North Central University	S. 9 th St. & Centennial Pl.	Restricted	108 stalls
R	Plumbers Union Local #15	S. 10 th St. & Park Ave S.	Restricted	51 stalls
S	Smith Brothers Lot #2	S. 10 th St. & Chicago Ave S.	Public	113 stalls
T	Interstate Parking Lot	S. 10 th St. & Park Ave S.	Public	173 stalls
U	Normandy Lot	415 South 9 th Street	Public	207 stalls
V	Nrg Energy Center Parking Garage	324 South 9 th Street	Public	500 stalls
W	Centre Village Ramp	700 5 th Avenue S.	Public	1208 stalls
X	Hennepin County Medical Center Ramp	614 S. 7 th Street	Public	1200 stalls
Y	"Hospital" Parking Ramp	812 S. 9 th Street	Public	1300 stalls
TOTAL (approximate)				7,466 stalls (460 restricted)

(Source: Parking Information from various websites, including Allied parking, Impark, HCMC.)

The KA Block Redevelopment will displace approximately 300 stalls. While no parking occupancy study was required as part of this analysis, it appears that there are several thousand off-street parking stalls available in the area. Further, there are on-street parking stalls available along each side of the KA Block as well as throughout the surrounding area.

3.2 – ON-SITE PARKING

The KA Block Redevelopment will provide structured off-street residential, employee and customer parking. Two levels of underground parking are proposed with each level proposed to house 265 parking stalls, thus totaling 530 off-street parking stalls for the development.

Table 3-2 illustrates the results of a parking analysis of the projected uses for the site. Because of the mix of uses, the City’s Municipal Code, Article III – Specific Off-Street Parking Requirements must be used to define minimum and maximum number of stalls allowed. The Base Zoning pertains to the rate at which the parking requirement is calculated. The Applied Zoning calculates the numbers of stalls required for each use.

Table 3-2: Minneapolis Zoning Code Parking Requirement

Land Use	Proposed DUs, Rooms, ksf or Occupants	Base Zoning		Applied Zoning	
		Minimum	Maximum	Minimum	Maximum
High Rise Apartment	306 units	*	1.6	6 stalls	490 stalls
Office (KA HQ)	95 ksf	0	1/1000	0 stalls	95 stalls
Office (Finnovation)	12 ksf	0	1/1000	0 stalls	12 stalls
Hotel	148 rooms	0	1/room	0 stalls	148 stalls
Hotel dining/mtg space	8,400 sf	0	30% capacity	0 stalls	168 stalls
Brewery (Production/ Processing/Storage)	12 ksf	0	1/1500	0 stalls	8 stalls
Event Center	13 ksf	0	30% capacity	0 stalls	260 stalls
TOTAL				6 stalls	1,181 stalls

* Minimum 1 guest stall per 50 dwellings

Therefore, the off-street parking being proposed for the KA Block Redevelopment will be approximately 50% of the maximum allowed by the City in their Off-Street Parking Requirements.

3.3 – COMPARISON WITH ITE PARKING GENERATION RATES

The Institute of Transportation Engineers (ITE) publication Parking Generation, 4th Edition, provides parking rates and equations for peak parking demand based on land use. Table 3-3 below lists the parking generation that was calculated based on the types and densities of land uses proposed for the KA Block Redevelopment.

Table 3-3: Estimated Parking Requirements per ITE

Land Use	ITE Code	Size	ITE Parking Rate or Equation	Weekday Peak Period Parking Demand
High Rise Apartment	222	306 units	Equation	448 stalls
Office (KA headquarters)	701-Urban	95 ksf	Equation	163 stalls
Office (Finnovation)	701-Urban	12 ksf	Equation	49 stalls
Hotel (incl. dining/mtg rooms)	310-Urban	148 rms.	Rate	148 stalls
Brewery (Prod. /Proc./Stor.)	140*	11.8 ksf	Rate	12 stalls
Event Center	595**	750 attn.	Rate	233 stalls
Tap Room (HTO Restaurant)	932 [†]	1.25 ksf	Rate	17 stalls
TOTAL				1,070 stalls

* The rate for Land Use 140 Manufacturing (1.02 veh/ksf) was used since ITE Parking Generation contained no parking rates for Brewery.

** The rate for Land Use 595 Convention Center (0.31 veh/attendee) was used since ITE Parking Generation contained no parking rates for Event Center.

† The rate for Land Use 932-Urban High-Turnover (Sit-Down) Restaurant (with Bar or Lounge) was used ITE Parking Generation contained no parking rates for Tap Room.

As was seen in Table 3-1, there are several pay parking lots in the immediate area that will be able to accommodate the additional demand. Further, because the development is located in the downtown area, numerous trips can be accommodated by transit, pedestrian, bicycle and taxi use.

3.4 – BICYCLE PARKING

Table 3-4 outlines the minimum bicycle parking required for this development by the City of Minneapolis and the amount of bicycle parking being provided by the development.

Section 541.180(c) of the Municipal Code states, “[d]evelopments with five hundred thousand (500,000) square feet of new or additional gross floor area in downtown districts shall provide bicycle parking and bicycle facilities as required by Chapter 549, Downtown Districts. All other developments in the downtown districts shall provide one (1) secure bicycle parking space for every twenty (20) automobile spaces provided, but in no case shall fewer than four (4) or more than thirty (30) bicycle parking spaces be required. For the purposes of this section, a secure bicycle parking space shall include a bicycle rack which permits the locking of the bicycle frame and one (1) wheel to the rack, and which supports the bicycle in a stable position without damage to wheels, frame or components. Residential uses in the downtown districts are subject to the requirements of Table 541-3, Bicycle Parking Requirements” (as shown below in Table 3-4).

Table 3-4: Bicycle Parking Requirements per City Code

Land Use	Units	Requirement	Required Spaces from Table 541-3 - Bicycle Parking Requirements	Number of Bicycle Stalls Being Provided by this Development
Apartment	306 units	1 space/2 dwelling units (not less than 90% shall be long term)	153 (138 long term; 15 short term)	306 (276 long term; 30 short term)
Office (KA)	95 ksf	3 spaces or 1 space per 15 ksf of GFA, whichever is greater (not less than 50% shall be long term)	6 (3 long-term; 3 short-term)	6 (3 long-term; 3 short-term)
Office (Finn)	12 ksf	3 spaces or 1 space per 15 ksf of GFA, whichever is greater (not less than 50% shall be long term)	3 (2 long-term; 1 short-term)	6 (3 long-term; 3 short-term)
Hotel	148 rms.	No bike parking requirement for this use	0	0
Hotel dining/mtg space	8,400 sf	3 spaces (not less than 50% shall be short term)	3 (2 long-term; 1 short-term)	3 (1 long-term; 2 short-term)
Brewery *	12 ksf	2 spaces or 1 space per 20,000 sq. ft. of GFA, whichever is greater (not less than 50% shall be long term)	2 (1 long-term; 1 short-term)	8 (4 long-term; 4 short-term)
Event Center	13 ksf	No bike parking requirement for this use	0	12 (2 long-term; 10 short-term)
TOTAL			167 (146 long term; 21 short term)	341 (289 long term; 52 short term)

*Assume Limited Production & Processing Industrial Use

It is noted that the City has experienced extensive bike parking demands being generated by brew pubs. Nevertheless, there is no minimum parking requirement at this point. To address this demand, additional convenience bike racks will be located along 5th Avenue South, and in the internal courtyard. The plan will show 8 loops out on 5th Avenue in front of the Brewery and 8 in the courtyard. These can park 2 bikes per loop. These bike racks will be an asset for visitors coming to the site.

The City has stipulated that for “unique” developments (e.g., convention center) an original bike parking forecast shall be conducted to attempt to estimate a more accurate actual demand. In this case, for 13 ksf event center space, twelve (12) spaces are committed for bicycle parking, with not less than ten (10) spaces being short term bicycle parking.

3.5 – LOADING SPACE

City Code specifies loading spaces based on all land uses within a development. Table 3-5 lists the loading space requirements by land use per code and the loading spaces proposed to be provided on site.

Table 3-5: Loading Space Requirements per City Code

Land Use	Code Requirement	Provided On Site
Residential	1 large (12'x50') or 2 small (2 x 10'x25')	1 large (12'x50')
Office	2 large (2 x 12'x50')	2 large (2 x 12'x50')
Hotel	2 large (2 x 12'x50')	1 large (12'x50')
Event Center	1 small (12'x50')	1 large (12'x50')
Food & Beverage	None	None

Figure 3-2 illustrates the proposed location and size of the loading spaces on the site plan. Residential loading will be located adjacent to the residential move-in elevator and trash room. One large space is being provided. Office loading will be located on the south side of the building. Two large spaces are being provided. Hotel loading will be provided on the east end of the building. One large space is being provided. The brewery/event center loading will be provided on the east side of the building. One large space is being provided.

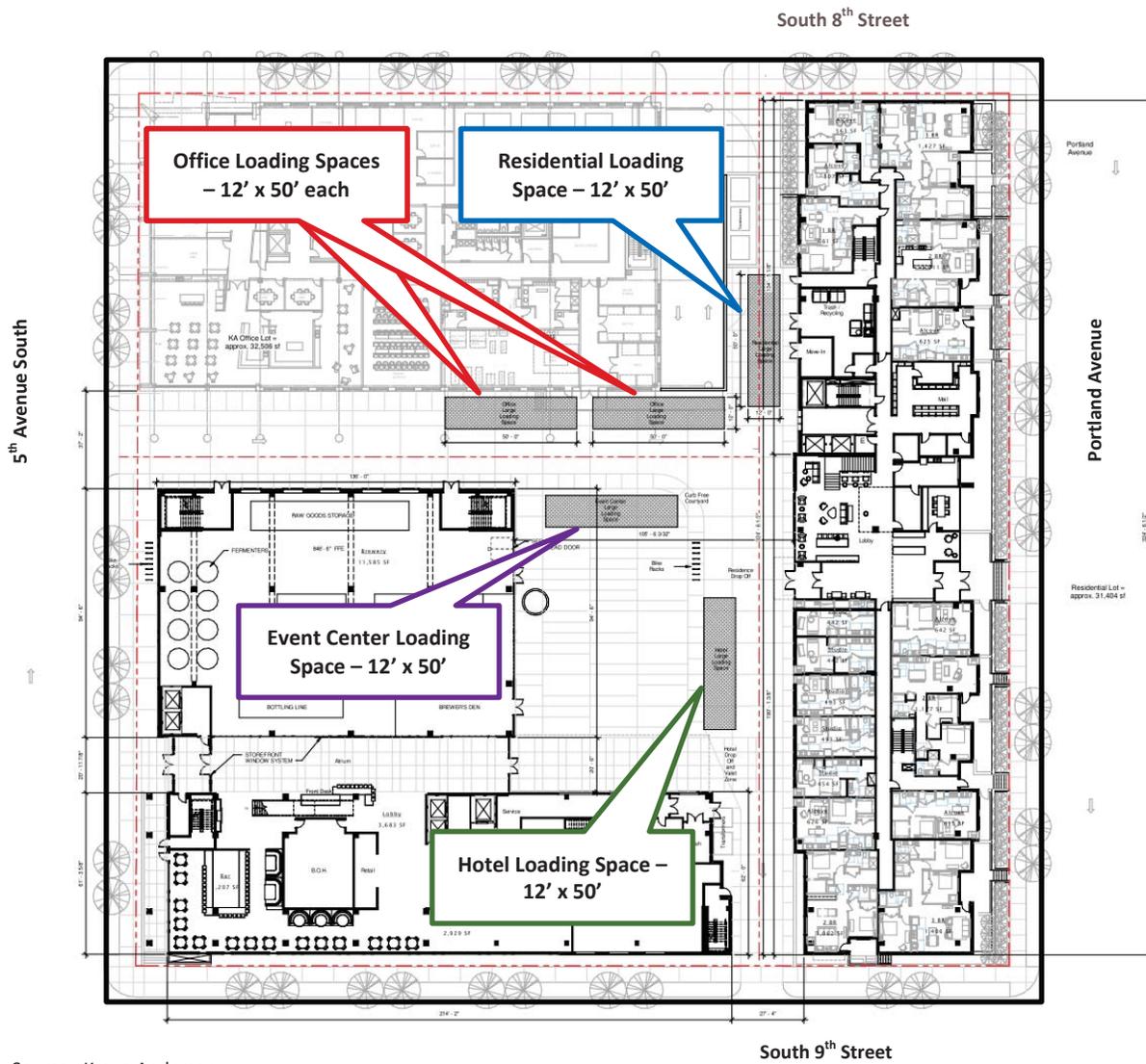
3.6 – VALET PARKING

The valet area is located in the internal courtyard south of the hotel loading area, and opposite the east entrance to the hotel. Valets will take customers' automobiles off site to a parking area to be contracted.

3.7 – PARKING SECURITY

KA will be using a parking access control and security system that will require proper credentials to be presented to gain entry into the parking garage. Credentials will be in the form of a proximity card or key fob for office employees and apartment residents. All other parking garage visitors will be permitted to gain access by taking a parking ticket upon entry and paying a parking fee when exiting.

Figure 3-2: Proposed Loading Locations



Source: Kraus-Anderson

4.0 – TRAFFIC CONSIDERATIONS

Table 4-1 illustrates the estimated trip generation calculated for the proposed conditions using the Institute of Transportation Engineers' (ITE's) Trip Generation Manual, Ninth Edition. The numbers shown do not reflect a reduction for shared trips, nor do they reflect any pass-by reduction, in order to provide a conservative comparison.

Table 4.1: Trip Generation Estimates for Proposed Land Use¹

Lane Use (according to Site Plan)	Size	Unit	ITE Land Use	ITE Land Use Code	Gross Trip Generation Estimates				
					Daily	AM Peak Hour		PM Peak Hour	
						In	Out	In	Out
Hotel	148	Rooms	Hotel	310	1,210	46	32	45	44
Event Center	13	ksf	Event Center	*	260	32	4	61	8
Apartments	306	units	High-Rise Apartment	222	1,286	23	69	65	42
KA Headquarters	95	ksf	Single Tenant Office	715	1,106	152	19	25	141
Hotel Dining	200	seats	Quality Restaurant	981	931	3	3	35	17
Finnovation	12	ksf	Gen. Office Bldg.	710	132	16	2	3	15
Brewery	11.58	ksf	Light Industrial	110	82	10	1	1	10
Taproom	1.25	ksf	Tap Room	**	83	n.a.	n.a.	56	27
Total					5,090	282	130	291	304
						412		595	

Source: Westwood, October 15, 2015

¹ Rates and equations based on ITE Trip Generation Manual, Ninth Edition, 2012.

* Rate not found in ITE Trip Generation Manual. Used 20 trips/ksf, as described below.

** Rate not found in ITE Trip Generation Manual. Used peak capacity estimation to determine peak hour trips, as described below

There are certain assumptions made about these trip generation totals:

- According to the ITE Trip Generation Manual, Land Use 310 “Hotel” can include “...places of lodging that provide sleeping accommodations and supporting facilities such as restaurants, cocktail lounges, meeting and banquet rooms or convention facilities, limited recreational facilities (pool fitness room), and/or other retail and service shops.”
- It is the intent of the developer to bring in a name-brand restaurant to the hotel site. Therefore, the trip generation for the Quality Restaurant was added separately.
- The ITE Trip Generation Manual, 9th Edition does not present rates or equations for event centers. A web search showed traffic impact studies for two event centers (Noah’s Event Venue in Sugar Land, TX, and The Ridge Event Center in Orem, UT). Both event centers were approximately 10,000 sq. ft.

The traffic studies conducted for each had varying trip generation rates:

- The Sugar Land event center used a rate of 24 daily trips for every 1,000 sq. ft., as well as 2.8 trips/1000 sq. ft. in the AM peak hour and 5.3 trips/1000 sq. ft. in the PM peak hour.³
- The Orem event center traffic study did not present a daily trip rate, but assumed a rate of 16.6 trips/1000 sq. ft. in the weekday PM peak hour. This rate was based on a survey of comparable site traffic resulting in an estimation that 20 sq. ft./person is needed during a seated event and an average of 3 persons per vehicle, thus generating the PM peak hour rate.⁴
- Further, the Orem study provided estimations of 88% inbound and 12% outbound trip distribution.

Therefore, for the Brewtel's event center, a median rate of 20 trips/1000 sq. ft. was used. Further, the AM and PM peak hour rates from the Sugar Land study were used.

- There are no rates or data for micro-breweries, brewpubs or breweries listed in the ITE Trip Generation Manual. In addition, there are very few traffic impact studies found on the internet – and of those, the micro-breweries are in rural settings and the trip generation is based on barrels produced per year. Therefore, the brewery was classified as General Light Industrial for the sake of this analysis.
- As with micro-breweries, there are no rates listing in the ITE Trip Generation Manual for tap rooms. Of the few traffic impact studies for micro-breweries found on the internet, some did discuss having tap rooms, but no specific rates were disclosed. Therefore, an estimation of this facility's patronage and trip behavior were made:
 - Assuming the tap room's size of 1,250 sq. ft., and dividing it by 15 sq. ft. per person, the resulting occupancy is 83 people. Therefore, assuming full occupancy, a conservative estimate of 83 trips would be generated during the PM peak hour.
 - To estimate directional distribution, the inbound and outbound rates for ITE Lane Use Code 925 – Drinking Place were used; e.g., 67% inbound and 33% outbound in the PM peak hour. This translates into 56 trips inbound and 27 trips outbound.

The City of Minneapolis has established modal shift goals that reflect the reduction in single occupancy vehicle trips and the reliance on other modes (e.g., transit, biking and walking) to provide transportation in the downtown area. These goals are listed in Table 4-2.

³ Donald R. Glenn, P.E., "Trip Generation of Noah's of Sugar Land", report prepared for the City of Sugar Land, Reynolds, Smith and Hills, Inc., Houston, TX, 01/21/2013.

⁴ "The Ridge Event Center Traffic Impact Study", report prepared by Horrocks Engineers for the City of Orem, UT, August 9, 2010.

Table 4-2: Modal Split Goals

Mode Split	Goal
Auto	40%
Transit	50%
Bike/Walk	10%

By meeting these goals, the resulting trip generation volumes for vehicular traffic may be reduced by 60%. Table 4-3 illustrates the resulting trip generation for the site.

Table 4.3: Vehicular Trip Generation Estimates Assuming Modal Split

Lane Use (according to Site Plan)	Size	Unit	ITE Land Use	ITE Land Use Code	Gross Trip Generation Estimates				
					Daily	AM Peak Hour		PM Peak Hour	
						In	Out	In	Out
Hotel	148	Rooms	Hotel	310	484	18	13	18	18
Event Center	13	ksf	Event Center	*	104	13	2	24	3
Apartments	306	units	High-Rise Apartment	222	514	9	28	26	17
KA Headquarters	95	ksf	Single Tenant Office	715	442	61	8	10	56
Hotel Dining	200	seats	Quality Restaurant	981	372	1	1	14	7
Finnovation	12	ksf	Gen. Office Bldg.	710	53	6	1	1	6
Brewery	11.58	ksf	Light Industrial	110	33	4	0	0	4
Taproom	1.25	ksf	Tap Room	**	33	n.a.	n.a.	22	11
Total					2,036	113	52	116	122
						165		238	

(Source: Westwood, February 22, 2016)

Westwood analyzed several intersections around the KA Block site, and assessed for traffic operational performance. Intersections analyzed included:

- South 8th Street and Fifth Avenue South
- South 9th Street and Fifth Avenue South
- South 8th Street and Portland Avenue
- South 9th Street and Portland Avenue

A full discussion of traffic operation is provided in the Traffic Impact Study found in the Appendix of this TDMP. Briefly, the operational analysis from that study indicates that the street intersections and the proposed access intersections will operate at LOS-D or better.

Two time frames were analyzed – Short-term (2018 Build and No-Build) representing the year construction is completed, and Long-term (2035 Build and No-Build) representing twenty years hence.

The results of the traffic operations analysis indicates that the access and the adjacent intersections will operate at acceptable levels of service for the 2018 and 2035 Build conditions respectively (see Traffic Impact Study). Lane group delay and 95th percentile queue lengths will be at or better levels than were recorded for existing conditions. There are times when the through street traffic blocks the access driveways to and from the KA Block development, but those times are brief and are common among many driveways and parking ramp accesses in the downtown area.

Regarding the modeling of these future conditions, signal timings were optimized to reflect the best possible traffic operation at the signalized intersections. This is consistent with the City of Minneapolis' efforts to retune signals on a regular basis.

Full traffic performance and queuing results appear in the Technical Appendix of the Traffic Impact Study.

5.0 – TRAVEL DEMAND MANAGEMENT STRATEGIES

5.1 – CITY OF MINNEAPOLIS TRANSPORTATION GOALS

The City of Minneapolis has developed a Ten-Year Transportation Action Plan that provides a vision of the future that states, “Minneapolis will build, maintain and enhance access to multi-modal transportation options for residents and business through a balanced system of transportation modes that supports the city’s land use vision, reduces adverse transportation impacts, decreases the overall dependency on automobiles, and reflects the city’s pivotal role as the center of the regional transportation network.” – The Minneapolis Plan for Sustainable Growth, (2008).

From this has emerged their “Transportation Vision for Minneapolis”:

- Transportation is important to the economic viability of the city, the region and the state. *Access Minneapolis* will lay the transportation groundwork for achieving the long-range vision of Minneapolis as a vital and thriving metropolitan urban center that is a great place to live, work, play, visit and conduct business.
- The city must remain livable and walkable to maintain its regional and national competitiveness. In most cases, it is not feasible or desirable to increase the curb-to-curb width of roadways in the city. However, there are many opportunities for improving the operational capacity of the transportation system without street widening. *Access Minneapolis* will result in a city that is livable and walkable while optimizing the operational capacity of the transportation system.
- *Access Minneapolis* will result in a citywide transportation system that is multi-modal (pedestrian, bicycle, transit, automobile, freight), providing good transportation choices to people, including people with disabilities.
- *Access Minneapolis* will result in a citywide transportation system that serves anticipated employment and residential growth and optimizes access to destinations by all modes (pedestrian, bicycle, transit, automobile, freight) throughout the city, between neighborhoods, to/from and within downtown.
- Although all modes of transportation are important, transit is critical for maximizing the people carrying capacity of the transportation system. *Access Minneapolis* will result in a transit system that operates efficiently and effectively in downtown and throughout the city. Transit will become the mode of choice for Minneapolis residents, workers and visitors.

With this vision in mind, the City of Minneapolis has developed Transportation Policies from “The Minneapolis Plan for Sustainable Growth” (2008). Key goals of this include:

- Building the City through multi-modalism
- Developing modal priorities in a neighborhood context
- Creating a walkable city
- Making transit more effective
- Creating a bicycle-friendly city
- Managing vehicle traffic
- Managing freight movement
- Managing Parking
- Developing funding and pricing strategies
- Supporting a vibrant multi-modal Downtown

5.2 – CITY OF MINNEAPOLIS TRANSPORTATION POLICY POINTS

The following policy points for transportation are included in Chapter 2 of the Minneapolis Plan for Sustainable Growth⁵:

- Policy 1: Encourage growth and reinvestment by sustaining the development of a multi-modal transportation system.
- Policy 2: Support successful streets and communities by balancing the needs of all modes of transportation with land use policy.
- Policy 3: Encourage walking throughout the city by ensuring that routes are safe, comfortable, pleasant, and accessible.
- Policy 4: Make transit a more attractive option for both new and existing riders.
- Policy 5: Ensure that bicycling throughout the city is safe, comfortable and pleasant.
- Policy 6: Manage the role and impact of automobiles in a multi-modal transportation system.
- Policy 7: Ensure that freight movement and facilities throughout the city meet the needs of the local and regional economy while remaining sensitive to impacts on surrounding land uses.

⁵ http://www.ci.minneapolis.mn.us/cped/docs/02_Transportation_100209.pdf

Policy 8: Balance the demand for parking with objectives for improving the environment for transit, walking and bicycling, while supporting the city's business community.

Policy 9: Promote reliable funding and pricing strategies to manage transportation demand and improve alternative modes.

Policy 10: Support the development of a multi-modal Downtown transportation system that encourages an increasingly dense and vibrant regional center.

Policy 11: Minneapolis recognizes the economic value of Minneapolis-St. Paul International Airport and encourages its healthy competition to reach global markets in an environmentally responsible manner.

5.3 – GOAL OF THE TRAVEL DEMAND MANAGEMENT PLAN

To succeed, this Travel Demand Management (TDM) plan must assist the City of Minneapolis to achieve their transportation goals. Based on previous TDM Plans in the area and the types of proposed land uses, the modal split goals for the project have been identified by the developer, as shown in Table 4-2.

The owners and/or TDM Liaison will work to achieve a mode share goal percentage of 60% non-single-occupant-vehicles for the residential development, as identified by the City of Minneapolis.

5.4 – SPECIFIC TRAVEL DEMAND MANAGEMENT STRATEGIES

This section outlines specific Travel Demand Management strategies to be implemented by the owner/end user/property manager/etc. of this site. The strategies detail the responsibilities of the site's responsible party in addressing the issues regarding transportation cited above. Kraus-Anderson, Incorporated, or their successors, by accepting the responsibility of implementing the items below, desire to help Minneapolis to achieve their goals of enhancing the local transportation system. Implementation of the items noted will help to encourage use of alternate modes of travel, enhance pedestrian friendliness, and achieve a balance in the needs of all users of the transportation system. Kraus-Anderson, Incorporated, or their successors specifically commits to the implementation of the following measures:

General

1. The owners and/or property managers of the development commit to hosting sessions where TMO, Metro Transit and shared bike and shared car providers would come to the site to conduct a commuter fair and educate employees on commuting options.

2. The owners and/or property managers of the development will appoint designated TDM Liaisons to coordinate the various TDM strategies that require ongoing attention. The responsibilities of the TDM Liaison would include upkeep of transit information and other communications, carpool program coordination, and administration of a shared car program.
3. The owners and/or property managers of the development will provide Real Time Monitors with transit information in the hotel, the office building and the residential building. This is of value to employees, residents and guests of the project. Information would include items such as transit schedules, Metro Transit commuter/ carpool program information (Rideshare and the Guaranteed Ride Home), NiceRide MN and/or bicycle/pedestrian commuter information or maps.
4. The owners and/or property managers of the development will assemble and disseminate a move-in package for all new residents. The move-in package will include all the pertinent information available at no cost on travel information such as parking, alternate modes of travel, bus routes, NiceRide MN, car sharing options and bike routes.
5. The owners and/or property managers of the development will provide each resident and office employee a link to the Downtown Minneapolis Transportation Management Organization's Commuter Connection webpage, <http://www.commuter-connection.org/>, that provides a host of links to transit, biking, LRT, rideshare and walking opportunities in Downtown Minneapolis.
6. The owner/TDM Liaison of the hotel will maintain commuter information in the lobby area for hotel guests. Information should include items such as transit schedules, information or maps identifying nearby bus stops and LRT stations, NiceRide MN and bicycle/pedestrian route information or maps.
7. The owner/TDM Liaison of the office buildings will maintain commuter information in the common areas and break rooms for employees. Information should include items such as transit schedules, Metro Transit commuter/carpool program information (Rideshare and the Guaranteed Ride Home), NiceRide MN and bicycle/pedestrian commuter information or maps.

Transit/Carpool

1. The owners/property managers commit to providing information on shared car services that are accessible to the public. The "HOURCAR" program, detailed at www.hourcar.org, is an example of such a program that is available in downtown Minneapolis. Other shared vehicle providers, such as Car2Go (www.car2go.com/en/minneapolis/) Enterprise CarShare (<https://www.enterprisecarshare.com/us/en/home.html>) and Zipcar (www.zipcar.com/minneapolis/find-cars) offer vehicles at sites throughout Minneapolis.

2. The property manager/TDM liaison for the apartment building will manage and disseminate shared-car formation to the residents and employees. K-A understands that such a program is valuable to those residents who may not have a personal vehicle, and who from time to time need to use a personal vehicle.
3. To maintain an awareness of alternative modes of transportation to office employees, the owners/property managers will distribute information through e-mail, flyers, posters in frequented locations, etc. This information will also be provided in the offices, locker area, or break rooms in the office or retail areas.
4. The owners and/or property managers of the development will distribute information on Mn/DOT's real-time traveler information program: 5-1-1 or www.511mn.org.
5. To maintain an awareness of alternative modes of transportation to office employees, information may be distributed through e-mail, flyers, posters in frequented locations, etc. This information will also be provided in the offices, locker area, or break rooms in the office or retail areas.
6. Residents and employees will be informed of Met Transit's "Go-To Card" passes for hassle-free transit. The link www.metrotransit.org/passes-go-to-cards.aspx will be provided to residents at move-in, or upon orientation for new hires.

Bicycles

1. The owners/property managers commit to provide a total of 341 bicycle parking spaces. The bicycle commitment is 289 long-term bicycle stalls to be provided within the buildings for use by residents, customers, employees or visitors, with an additional 52 short-term spaces available for residents, customers, employees or visitors. This bike parking commitment is broken down by land use and is shown on Table 3-4 of this report.
2. The owners/property managers will actively promote biking as a mode of transportation to and from the site by providing outdoor bicycle parking spaces and a repair station for patrons, residents and employees within the indoor bicycle storage space. The bike repair station will be in a room on the P2 level of parking and accessible to residents and office employees.
3. The owners/property managers will provide maps and information to direct riders through the area and to adjacent bicycle trails.
4. The owners/property managers will provide bike shelters and racks at main entrances to public buildings and in proximity to accesses. The developer will work with the hotel and brewery management to determine the best locations for such bike facilities.

5. The owners/property managers will promote Nice Ride MN to employees, residents and visitors in the development site. NiceRide MN Stations exist near the K-A block – one at the corner of 4th Avenue South & South 9th Street; and another on 4th Avenue South between 6th and 7th Streets South. Other nearby Nice Ride MN stations are shown on Figures 2-2 and 2-4.
6. The owners/property managers will actively promote biking by providing shower/locker facilities for KA employees who commute via bicycle.

Deliveries

1. The owners/property managers will develop and maintain a policy that encourages truck and service deliveries to occur outside of peak traffic times. As a goal, 80% of truck and service deliveries will occur before noon, which is outside the peak hour. This would not include FedEx/UPS-type deliveries.

Parking

1. The owners/property managers will use appropriate signage to designate parking spaces for employees versus residents and hotel patrons.
2. The owners/property managers will be using a parking access control and security system that will require proper credentials to be presented to gain entry into the parking garage. Credentials will be in the form of a proximity card or key fob for office employees and apartment residents. All other parking garage visitors will be permitted to gain access by taking a parking ticket upon entry and paying a parking fee when exiting.
3. The owners/property managers will apply a residential parking ratio that is less than one-to-one, as this site is taking advantage of nearby bus lines and the LRT stations that are five blocks away at Government Plaza or at Downtown East Station.
4. Residential Parking will not be free. Residents will not be required to lease parking, but those who chose to do so will have reserved spaces in the ramp. The owners/property managers will apply a parking fee that will be market rate for downtown residential parking and will be a lease contract separate from apartment lease. Residential unit renters or homeowners' association will not lease or sell any residential parking stalls to any person other than a resident or tenant of the building.
5. The owners/property managers will meet the guest parking code requirement of 1 space per 50 DUs. Six dedicated guest parking spaces will be located on the P2 residential parking level. Guest will check in with the front desk in order to park in these spaces and get validation to exit the parking garage.

6. Brewery customers, hotel check-in/check-out and valet, additional residential guests/visitors and delivery services will have access to the internal drive on the ground floor of the buildings. No long-term internal drive parking will be provided. Short-term parking will be provided at the discretion of the property manager/owner within the P1 parking area.
7. Information regarding on-street parking and nearby public pay lots and ramps will be held by the hotel concierge and office manager for information from guests or employees and visitors.

Resident Surveys and TDMP Plan Status Reports

1. With the assistance of Commuter Connection, the owners/property managers shall conduct a baseline resident commuting survey within the first 6 months after 50% occupancy of the site. The owners/property managers will continue to conduct this survey every two years after that, for ten years or until the TDM Plan mode split goals are achieved.

**TRAVEL DEMAND MANAGEMENT PLAN
KRAUS-ANDERSON BLOCK REDEVELOPMENT
525 SOUTH 8TH STREET
MINNEAPOLIS, MN**

PLAN APPROVAL

Hennepin County Planning and Project Development Division

By: _____ Dated: _____

Bruce Engelsma, Chief Executive Officer
Kraus-Anderson, Incorporated
525 South 8th Street
Minneapolis, MN 55404

Minneapolis Community and Economic Development Department

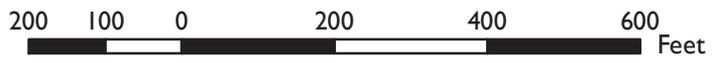
By: _____ Dated: _____

Steve Poor, CPED Development Services Director

Minneapolis Public Works Department

By: _____ Dated: _____

Steve Mosing, Traffic Operations Engineer



PROPERTY ADDRESS

501, 507, 515 and 523 S 8th St, 502 and 518 S 9th St and 811 5th Ave S

FILE NUMBER

BZZ-7588

KA Block Redevelopment
 Exterior Materials
 ESG Architects
 February 12, 2016

Residential Building		
	Material	Percentage

East Elevation (Portland Ave)	Brick	18%
	Metal Panel 5A	17%
	Metal Panel 5B	12%
	Metal Panel 5C	9%
	Metal Panel 5D	5%
	Glazing	39%
		100%

South Elevation (9th Street)	Brick	28%
	Metal Panel 5A	14%
	Metal Panel 5B	18%
	Metal Panel 5C	2%
	Metal Panel 5D	2%
	Glazing	36%
		100%

West Elevation	Brick	17%
	Metal Panel 5A	18%
	Metal Panel 5B	21%
	Metal Panel 5C	5%
	Metal Panel 5D	0%
	Glazing	39%
		100%

North Elevation	Brick	20%
	Metal Panel 5A	18%
	Metal Panel 5B	14%
	Metal Panel 5C	9%
	Metal Panel 5D	0%
	Glazing	39%
		100%

Office Building

	Material	Percentage
--	----------	------------

East Elevation

Architectural Precast Wall Panel Buff	38%
Architectural Precast Wall Panel Gray	23%
Granite	0%
Prefinished Metal Wall Panel System	14%
Glazing	25%

	100%
--	------

South Elevation

(9th Street) Architectural Precast Wall Panel Buff	29%
Architectural Precast Wall Panel Gray	14%
Granite	4%
Prefinished Metal Wall Panel System	18%
Glazing	35%

	100%
--	------

West Elevation

(5th Street) Architectural Precast Wall Panel Buff	22%
Architectural Precast Wall Panel Gray	0%
Granite	10%
Prefinished Metal Wall Panel System	6%
Glazing	62%

	100%
--	------

North Elevation

Architectural Precast Wall Panel Buff	26%
Architectural Precast Wall Panel Gray	13%
Granite	4%
Prefinished Metal Wall Panel System	18%
Glazing	39%

	100%
--	------

Hotel Building

	Material	Percentage
--	----------	------------

East Elevation

Brick	59%
Metal Panel 5E	0%
Composite Metal Panel	9%
High Pressure Laminate Panel	0%
Glazing	32%

	100%
--	------

South Elevation

(9th Street) Brick	37%
Metal Panel 5E	17%
Composite Metal Panel	8%

High Pressure Laminate Panel	4%
Glazing	34%

	100%
--	------

West Elevation	Brick	42%
(5th Street)	Metal Panel 5E	2%
	Composite Metal Panel	6%
	High Pressure Laminate Panel	13%
	Glazing	37%

	100%
--	------

North Elevation	Brick	42%
	Metal Panel 5E	11%
	Composite Metal Panel	14%
	High Pressure Laminate Panel	0%
	Glazing	33%

	100%
--	------

Brewery Event Center Finnovation Building		
---	--	--

	Material	Percentage
--	----------	------------

East Elevation	Metal Panel 5F	18%
	Metal Panel 5G	45%
	Architectural Precast Concrete Wall Panel	13%
	Glazing	24%

	100%
--	------

South Elevation	Metal Panel 5F	NA
(9th Street)	Metal Panel 5G	NA
	Architectural Precast Concrete Wall Panel	NA
	Glazing	NA

	NA
--	----

West Elevation	Metal Panel 5F	22%
(5th Street)	Metal Panel 5G	24%
	Metal Panel 5J	6%
	Architectural Precast Concrete Wall Panel	6%
	Glazing	42%

	100%
--	------

North Elevation	Metal Panel 5F	0%
	Metal Panel 5G	37%
	Architectural Precast Concrete Wall Panel	29%
	Glazing	34%

	100%
--	------



6" LED Surface Mount Cylinder

LD6LED3S

LD6LED4S

Wet Location
120V, 277V

DATE: _____ TYPE: _____

FIRM NAME: _____

PROJECT: _____



For conversion to millimeters,
multiply inches by 25.4
Not to Scale

APPLICATIONS:

LiteForms™ LD6LED3S & LD6LED4S Series is a 6" direct surface mount LED cylinder designed to combine superior brightness control with energy savings and long lamp life. Suitable for a variety of commercial, retail, and institutional applications to achieve illumination patterns on vertical walls or columns. Optical lens options in spot, medium, or wide distribution are ideal for higher ceiling applications such as transportation terminals or atrium areas. Suitable for use in ambient temperatures up to 30°C (86°F) during operation.

HOUSING:

Seamless .058" thick extruded aluminum cylinder body with durable powder coat paint finish in a variety of architectural colors including Prescolite's exclusive Zet, metallic silver.

REFLECTOR:

High purity spun aluminum self-flanged reflector with iridescence suppressed Alzak anodized finish retained by factory installed spring clips and safety cable for ease of maintenance. Provided with integral clear tempered glass lens to protect LEDs.

LED LIGHT ENGINE:

The LED light engines are equipped with (10) high brightness white LEDs (3500K or 4000K) on a metal clad circuit board. Secured to an integral die cast

aluminum heat sink for excellent thermal management. System designed for optimal life and lumen maintenance (50,000 hours at 70% average lumen maintenance*).

*LED life calculations are based upon maintaining application LED junction temperatures and drive currents at or below manufacturer's requirements per IESNA- LM-80-08 test data.

Optics: Vacuum metallized injection molded optical reflector features (10) parabolic LED modules with Prescolite's patented (U.S. Patent No. 6,254,256) American Matte™ finish to produce uniform illumination. Optional optical lenses provide a choice of spot, medium, or wide distribution for added versatility.

LED DRIVER:

Energy efficient solid state constant current electronic driver with 50,000 hour minimum anticipated life. Meets UL Class 2, inherent short-circuit protection, self limited, overload protected.

INSTALLATION:

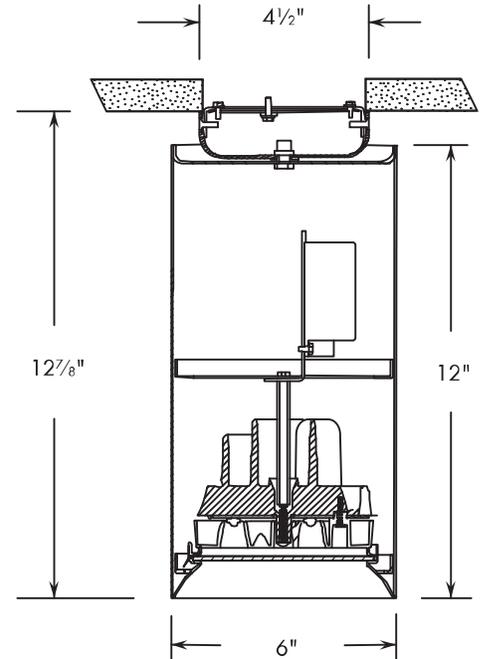
Easy installation onto standard J-Box.

CERTIFICATIONS:

CSA certified to US and Canadian safety standards. Suitable for wet locations.

WARRANTY:

5 year warranty



CATALOG NUMBER:

EXAMPLE: LD6LED4SW35K8FL35 277V Z

CYLINDER	LED COLOR	LED COLOR TEMP	CRI	BEAM ANGLE	VOLTAGE	REFLECTOR COLOR	CYLINDER FINISH
<input type="checkbox"/> LD6LED 6" LED cylinder LED GENERATION	<input type="checkbox"/> W White	<input type="checkbox"/> 35K 3500 Kelvin <input type="checkbox"/> 40K 4000 Kelvin	<input type="checkbox"/> 8 Nominal 80+ CRI	<input type="checkbox"/> Blank Vacuum metallized reflector with 45° cutoff <input type="checkbox"/> FL35 Lensed optic with 35° wide distribution <input type="checkbox"/> MD25 Lensed optic with 25° medium distribution <input type="checkbox"/> SP18 Lensed optic with 18° spot distribution	<input type="checkbox"/> Blank 120V only <input type="checkbox"/> 277V 120/277V REFLECTOR FINISH <input type="checkbox"/> Blank Specular <input type="checkbox"/> SS Semi-Specular	<input type="checkbox"/> Blank Clear Alzak <input type="checkbox"/> CG Champagne Gold Alzak <input type="checkbox"/> BL Black Alzak <input type="checkbox"/> WE Wheat Alzak <input type="checkbox"/> LW Light Wheat Alzak <input type="checkbox"/> PW Pewter Alzak <input type="checkbox"/> WH White Paint	<input type="checkbox"/> WH White <input type="checkbox"/> BL Black <input type="checkbox"/> Z Zet <input type="checkbox"/> BA Brushed Aluminum <input type="checkbox"/> BZ Bronze
<input type="checkbox"/> 3 14W, 700 Lumen LED <input type="checkbox"/> 4 28W, 1100 Lumen LED MOUNTING <input type="checkbox"/> S Surface							



A Division of Hubbell Lighting, Inc.

In a continuing effort to offer the best product possible we reserve the right to change, without notice, specifications or materials that in our opinion will not alter the function of the product.
Web: www.prescolite.com • Tech Support: (888) 777-4832

LFO-LED-012

PHOTOMETRIC DATA

LiteForms™™ - LD6LED - 6" Surface Mount LED Cylinder

DRIVER DATA	LD6LED3	LD6LED3 277V	LD6LED4	LD6LED4 277V
Input Voltage	120V +/- 10%	120/277V	120V +/- 10%	120/277V
Input Frequency	60 Hz	50/60 Hz	50/60 Hz	50/60 Hz
Input Current	0.13A	0.13A/0.07A	0.235A	0.23A/0.13A
Input Power	14W	15W	28W	27.5/28.5W
Constant Current Output	700mA	700mA	1400mA	1400mA
Power Factor	≥0.90	≥0.90	≥0.90	0.98/0.78
THD	<20%	<20%	<20%	<20%
EMI Filtering	FCC 47CFR Part 15, Class A	FCC 47CFR Part 15, Class B	FCC 47CFR Part 15, Class A	FCC 47CFR Part 15, Class B
Operating Temperature	-40°C to 60°C	-30°C to 60°C	-40°C to 60°C	-30°C to 60°C
Dimming	No	No	No	No
Over-voltage, over-current, short-circuit protected				

LD6LED3SW35K7

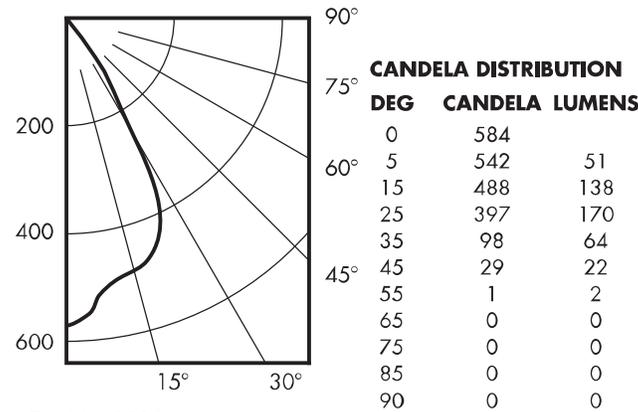
LED Light Engine: (10) LED Array 3500K Std. CRI
System Wattage: 13.8
Fixture delivered lumens: 448
Fixture Efficacy: 32.4
Spacing Criteria: 0.8

ZONAL LUMEN SUMMARY

ZONE	LUMENS	%LUMINAIRE
0-30	360	80.3
0-40	424	94.6
0-60	448	100.0
0-90	448	100.0
90-180	0	0.0
0-180	448	100.0

LUMINANCE DATA IN CANDELA/SQ. METER

Angle in Vertical	Average - 0°
45°	3996
55°	170
65°	0
75°	0
85°	0



Test No. 2918

Tested at 25°C Ambient in accordance to IESNA LM-79-2008

COEFFICIENTS OF UTILIZATION

Zonal Cavity Method

Room Cavity Ratio	% Effective Floor Cavity Reflectance																
	80%		70%		50%		30%		10%								
	20%	20%	20%	20%	20%	20%	20%	20%	20%	20%							
1	114	111	109	107	111	109	107	105	105	103	102	101	100	99	98	97	96
2	109	104	100	97	106	102	99	96	99	96	94	96	94	92	93	91	90
3	103	97	93	89	102	96	92	88	93	90	87	91	88	85	89	86	84
4	99	91	86	82	97	90	85	82	88	84	81	86	83	80	84	81	79
5	94	86	81	76	92	85	80	76	83	79	75	82	78	75	80	77	74
6	90	81	75	71	88	80	75	71	79	74	71	77	73	70	76	73	70
7	85	77	71	67	84	76	71	67	75	70	66	73	69	66	72	69	66
8	81	72	67	63	80	72	66	63	71	66	62	70	65	62	69	65	62
9	78	69	63	59	77	68	63	59	67	62	59	66	62	59	66	61	59
10	74	65	60	56	73	65	59	56	64	59	56	63	59	56	62	58	55

LD6LED3SW35K7

Test No. 2918

LD6LED3SW35K7FL35

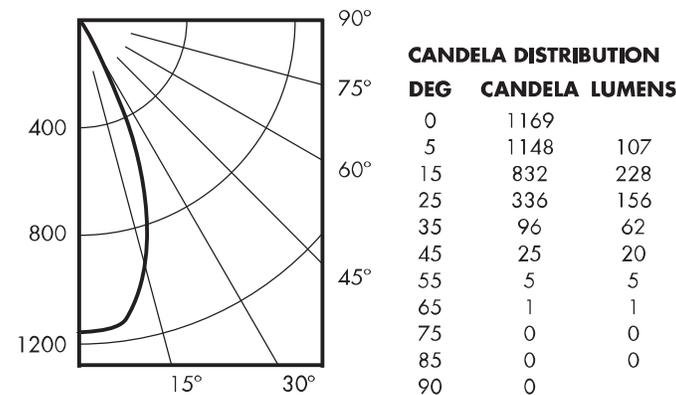
LED Light Engine: (10) LED Array 3500K Std CRI with Flood Lens
System Wattage: 13.8
Fixture delivered lumens: 579
Fixture Efficacy: 41.9
Spacing Criteria: 0.6

ZONAL LUMEN SUMMARY

ZONE	LUMENS	%LUMINAIRE
0-30	491	84.8
0-40	553	95.5
0-60	578	99.8
0-90	579	100.0

LUMINANCE DATA IN CANDELA/SQ. METER

Angle in Vertical	Average - 0°
45°	3444
55°	849
65°	231
75°	0
85°	0



Test No. 2919

Tested at 25°C Ambient in accordance to IESNA LM-79-2008

COEFFICIENTS OF UTILIZATION

Zonal Cavity Method

Room Cavity Ratio	% Effective Floor Cavity Reflectance																
	80%		70%		50%		30%		10%								
	20%	20%	20%	20%	20%	20%	20%	20%	20%	20%							
1	114	112	110	108	112	110	108	106	106	104	103	102	101	100	98	98	97
2	109	105	102	99	107	103	100	98	100	98	95	97	95	93	95	93	91
3	105	99	95	91	103	98	94	91	95	92	89	93	90	88	91	89	87
4	101	94	89	86	99	93	88	85	91	87	84	89	86	83	87	84	82
5	96	89	84	80	95	88	84	80	87	82	79	85	81	79	83	80	78
6	92	85	80	76	91	84	79	76	83	78	75	81	78	75	80	77	74
7	89	81	76	72	88	80	75	72	79	75	71	78	74	71	77	73	71
8	85	77	72	69	84	77	73	68	76	71	68	75	71	68	74	70	68
9	82	74	69	65	81	73	69	65	73	68	65	72	68	65	71	67	65
10	79	71	66	62	78	70	67	62	70	65	62	69	65	62	68	65	62

LD6LED3SW35K7FL35

Test No. 2919

NOTES: Refer to www.prescolite.com for additional photometric tests (IES Files).



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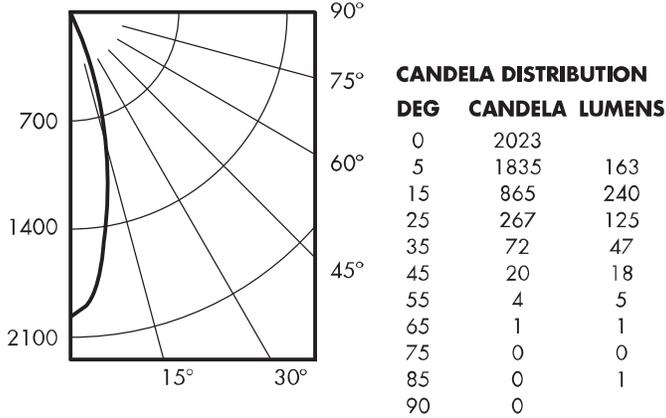


HUBBELL Lighting, Inc.

PHOTOMETRIC DATA

LD6LED3SW35K7MD25

LED Light Engine: (10) LED Array 3500K Std CRI with Medium Lens
 System Wattage: 13.8
 Fixture delivered lumens: 600
 Fixture Efficacy: 43.5
 Spacing Criteria: 0.4



Test No. 2920

Tested at 25°C Ambient in accordance to IESNA LM-79-2008

ZONAL LUMEN SUMMARY

ZONE	LUMENS	%LUMINAIRE
0-30	528	88.0
0-40	575	95.9
0-60	598	99.7
0-90	600	100.0
90-180	0	0.0
0-180	600	100.0

LUMINANCE DATA IN CANDELA/SQ. METER

Angle in Vertical	Average - 0°
45°	2756
55°	679
65°	231
75°	0
85°	0

COEFFICIENTS OF UTILIZATION Zonal Cavity Method

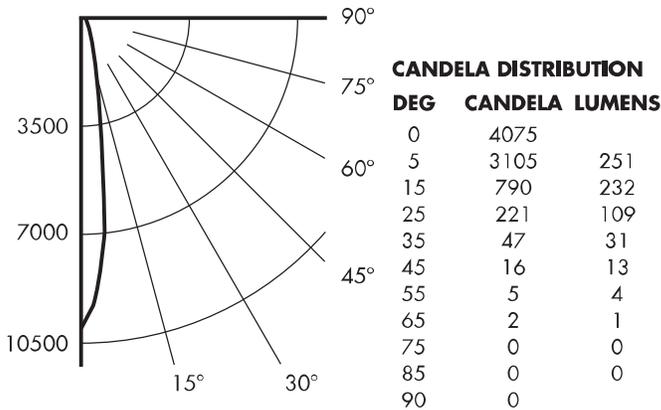
Room Cavity Ratio	% Effective Floor Cavity Reflectance																
	80%					70%					50%						
	20% Effective Floor Cavity Reflectance																
	% Wall Reflectance																
	70	50	30	10	70	50	30	10	50	30	10	50	30	10	50	30	10
1	114	112	110	108	112	110	108	106	106	105	103	102	101	100	99	98	97
2	110	106	103	100	108	104	101	99	101	99	97	98	96	95	96	94	93
3	106	101	97	93	104	99	96	93	97	94	91	95	92	90	92	90	89
4	102	96	92	88	100	95	91	88	93	89	87	91	88	86	89	87	85
5	98	92	87	84	97	91	87	83	89	85	83	88	84	82	86	83	81
6	95	88	83	80	94	87	83	79	86	82	79	84	81	78	83	80	78
7	92	84	80	76	91	84	79	76	83	79	76	81	78	75	80	77	75
8	89	81	76	73	88	81	76	73	80	76	73	79	75	72	78	75	72
9	86	78	74	70	85	78	73	70	77	73	70	76	72	70	75	72	70
10	83	75	71	68	82	75	71	68	74	70	68	74	70	67	73	70	67

LD6LED3SW35K7MD25

Test No. 2920

LD6LED3SW35K7SP18

LED Light Engine: (10) LED Array 3500K Std CRI with Spot Lens
 System Wattage: 13.8
 Fixture delivered lumens: 640
 Fixture Efficacy: 46.38
 Spacing Criteria: 0.3



Test No. 2921

Tested at 25°C Ambient in accordance to IESNA LM-79-2008

ZONAL LUMEN SUMMARY

ZONE	LUMENS	%LUMINAIRE
0-30	591	92.3
0-40	622	97.2
0-60	639	99.8
0-90	640	100.0
90-180	0	0.0
0-180	640	100.0

LUMINANCE DATA IN CANDELA/SQ. METER

Angle in Vertical	Average - 0°
45°	2204
55°	849
65°	461
75°	0
85°	0

COEFFICIENTS OF UTILIZATION Zonal Cavity Method

Room Cavity Ratio	% Effective Floor Cavity Reflectance																
	80%					70%					50%						
	20% Effective Floor Cavity Reflectance																
	% Wall Reflectance																
	70	50	30	10	70	50	30	10	50	30	10	50	30	10	50	30	10
1	115	113	111	109	113	111	109	107	107	105	104	103	102	101	100	99	98
2	111	107	104	102	109	106	103	101	103	101	99	100	98	97	97	96	95
3	108	103	99	96	106	101	98	95	99	96	94	97	94	93	95	93	91
4	104	99	95	92	103	98	94	91	96	93	90	94	91	89	92	90	88
5	101	95	91	88	100	94	90	87	93	89	87	91	88	86	90	87	85
6	98	92	87	84	97	91	87	84	90	86	84	88	85	83	87	85	83
7	95	89	84	82	94	88	84	81	87	84	81	86	83	81	85	82	80
8	93	86	82	79	92	85	82	79	85	81	78	84	81	78	83	80	78
9	90	83	79	77	89	83	79	76	82	79	76	82	78	76	81	78	76
10	88	81	77	74	87	81	77	74	80	77	74	80	76	74	79	76	74

LD6LED3SW35K7SP18

Test No. 2921

NOTES: Refer to www.prescolite.com for additional photometric tests (IES Files).

- 5 year warranty requires product registration. Warranty limited to repair and replacement of defective parts of the LED system and does not include labor or installation after first year. See www.prescolite.com for details.
- Operation in ambient temperatures higher than those specified may shorten life and will void warranty.



PHOTOMETRIC DATA

LD6LED4SW35K8

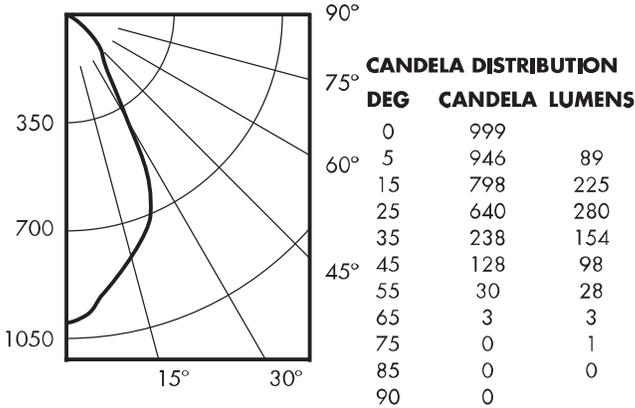
LED Light Engine: (10) LED Array 3500K Mid CRI
 System Wattage: 27.4
 Fixture delivered lumens: 877
 Fixture Efficacy: 32
 Spacing Criteria: 0.8

ZONAL LUMEN SUMMARY

ZONE	LUMENS	%LUMINAIRE
0-30	593	67.7
0-40	748	85.3
0-60	873	99.6
0-90	877	100.0
90-180	0	0.0
0-180	877	100.0

LUMINANCE DATA IN CANDELA/SQ. METER

Angle in Vertical	Average - 0°
45°	17635
55°	5096
65°	692
75°	0
85°	0



Test No. 2925

Tested at 25°C Ambient in accordance to IESNA LM-79-2008

COEFFICIENTS OF UTILIZATION Zonal Cavity Method

Room Cavity Ratio	% Effective Floor Cavity Reflectance																
	80%		70%		50%		30%		10%								
	70	50	30	10	70	50	30	10	50	30	10						
20% Effective Floor Cavity Reflectance																	
% Wall Reflectance																	
1	113	110	108	106	111	108	106	104	104	102	101	100	99	98	97	96	95
2	107	102	98	95	105	101	97	93	97	94	91	94	92	99	92	89	88
3	102	95	90	86	100	94	89	85	91	87	84	88	85	82	86	83	81
4	96	88	83	78	94	87	82	78	85	80	77	83	79	76	81	78	75
5	91	82	76	72	89	81	76	71	80	75	71	78	74	70	76	73	69
6	86	77	71	66	85	76	70	66	75	69	66	73	69	65	72	68	65
7	82	72	66	61	80	71	65	61	70	65	61	69	64	61	68	64	60
8	78	68	62	57	76	67	61	57	66	61	57	65	60	57	64	60	56
9	74	64	58	54	73	63	57	53	62	57	53	61	57	53	61	56	53
10	70	60	54	50	69	60	54	50	59	54	50	58	53	50	57	53	50

LD6LED4SW35K8

Test No. 2925

LD6LED4SW35K8FL35

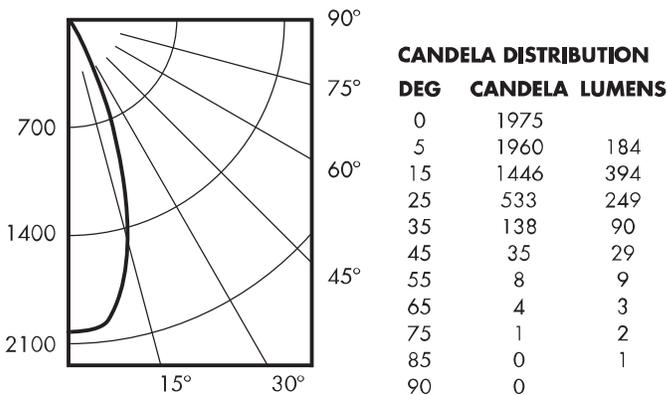
LED Light Engine: (10) LED Array 3500K Mid CRI with Flood Lens
 System Wattage: 27.4
 Fixture delivered lumens: 959
 Fixture Efficacy: 35
 Spacing Criteria: 0.6

ZONAL LUMEN SUMMARY

ZONE	LUMENS	%LUMINAIRE
0-30	826	86.1
0-40	916	95.5
0-60	954	99.5
0-90	959	100.0
90-180	0	0.0
0-180	959	100.0

LUMINANCE DATA IN CANDELA/SQ. METER

Angle in Vertical	Average - 0°
45°	4822
55°	1359
65°	922
75°	376
85°	0



Test No. 2924

Tested at 25°C Ambient in accordance to IESNA LM-79-2008

COEFFICIENTS OF UTILIZATION Zonal Cavity Method

Room Cavity Ratio	% Effective Floor Cavity Reflectance																
	80%		70%		50%		30%		10%								
	70	50	30	10	70	50	30	10	50	30	10						
20% Effective Floor Cavity Reflectance																	
% Wall Reflectance																	
1	114	112	109	107	112	110	108	106	106	104	103	102	101	99	98	98	97
2	109	105	102	99	107	103	100	98	100	98	95	97	95	93	95	93	91
3	105	99	95	92	103	98	94	91	95	92	89	93	90	88	91	89	87
4	101	94	89	86	99	93	89	85	91	87	84	89	86	83	87	85	82
5	97	89	84	81	95	89	84	80	87	83	80	85	82	79	84	81	78
6	93	85	80	76	91	84	80	76	83	79	76	82	78	75	81	77	75
7	89	81	76	73	88	81	76	72	80	75	72	78	75	72	77	74	71
8	86	78	73	69	85	77	72	69	76	72	69	75	71	69	74	71	68
9	83	75	70	66	82	74	69	66	73	69	66	72	68	66	72	68	65
10	80	72	67	63	79	71	66	63	70	66	63	70	66	63	69	65	63

LD6LED4SW35K8FL35

Test No. 2924

NOTES

Refer to www.prescolite.com for additional photometric tests (IES Files).

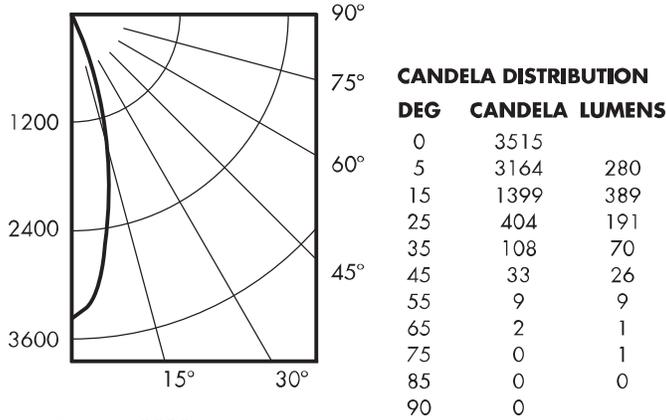


PHOTOMETRIC DATA

LiteForms™™ - LD6LED - 6" Surface Mount LED Cylinder

LD6LED4SW35K8MD25

LED Light Engine: (10) LED Array 3500K Mid CRI with Medium Lens
 System Wattage: 27.4
 Fixture delivered lumens: 967
 Fixture Efficacy: 35.2
 Spacing Criteria: 0.4



Test No. 2923
 Tested at 25°C Ambient in accordance to IESNA LM-79-2008

ZONAL LUMEN SUMMARY

ZONE	LUMENS	%LUMINAIRE
0-30	859	88.9
0-40	930	96.2
0-60	965	99.8
0-90	967	100.0
90-180	0	0.0
0-180	967	100.0

LUMINANCE DATA IN CANDELA/SQ. METER

Angle in Vertical	Average - 0°
45°	4547
55°	1529
65°	461
75°	0
85°	0

COEFFICIENTS OF UTILIZATION Zonal Cavity Method

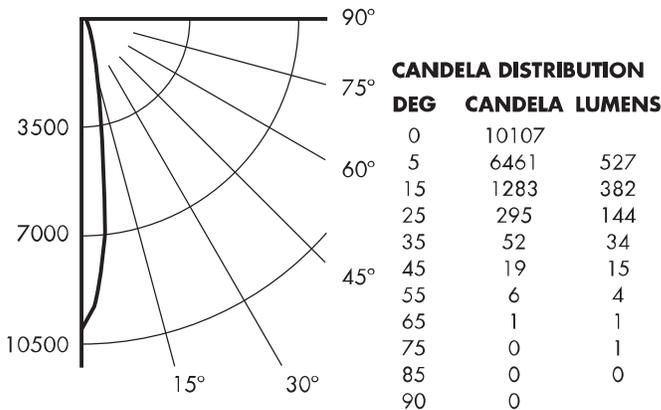
Room Cavity Ratio	% Effective Floor Cavity Reflectance																
	80%	70%	50%	30%	10%												
	20% Effective Floor Cavity Reflectance																
	% Wall Reflectance																
	70	50	30	10	70	50	30	10	50	30	10						
1	115	112	110	108	112	110	108	107	106	105	103	103	101	100	99	98	97
2	110	106	103	100	108	105	102	99	102	99	97	99	97	95	96	94	93
3	106	101	97	94	104	100	96	93	97	94	92	95	93	90	93	91	89
4	102	97	92	89	101	95	91	88	93	90	87	92	89	86	90	87	85
5	99	92	88	84	97	91	87	84	90	86	83	88	85	83	87	84	82
6	96	89	84	81	94	88	84	80	86	83	80	85	82	79	84	81	79
7	92	85	81	77	91	85	80	77	83	80	77	82	79	76	81	78	76
8	89	82	77	74	88	82	77	74	81	77	74	80	76	74	79	76	73
9	87	79	75	72	86	79	74	71	78	74	71	77	74	71	76	73	71
10	84	77	72	69	83	76	72	69	75	72	69	75	71	69	74	71	69

LD6LED4SW35K8MD25

Test No. 2923

LD6LED4SW35K8SP18

LED Light Engine: (10) LED Array 3500K Mid CRI with Spot Lens
 System Wattage: 27.5
 Fixture delivered lumens: 1108
 Fixture Efficacy: 40.2
 Spacing Criteria: 0.2



Test No. 2922
 Tested at 25°C Ambient in accordance to IESNA LM-79-2008

ZONAL LUMEN SUMMARY

ZONE	LUMENS	%LUMINAIRE
0-30	1052	95.0
0-40	1087	98.1
0-60	1106	99.8
0-90	1108	100.0
90-180	0	0.0
0-180	1108	100.0

LUMINANCE DATA IN CANDELA/SQ. METER

Angle in Vertical	Average - 0°
45°	2618
55°	1019
65°	231
75°	0
85°	0

COEFFICIENTS OF UTILIZATION Zonal Cavity Method

Room Cavity Ratio	% Effective Floor Cavity Reflectance																
	80%	70%	50%	30%	10%												
	20% Effective Floor Cavity Reflectance																
	% Wall Reflectance																
	70	50	30	10	70	50	30	10	50	30	10						
1	115	113	111	108	113	111	110	108	107	106	105	104	103	102	100	99	99
2	112	108	106	102	110	107	104	102	104	102	100	101	99	98	98	97	96
3	109	104	101	97	107	103	100	97	100	98	96	98	96	94	96	94	93
4	106	101	97	94	104	99	96	94	98	95	92	96	93	91	94	92	90
5	103	97	93	90	101	96	93	90	95	92	90	93	91	89	92	90	88
6	100	94	91	88	99	94	90	88	92	89	87	91	89	86	90	88	86
7	98	92	88	85	97	91	88	85	90	87	85	89	86	84	88	86	84
8	95	89	86	83	95	89	85	83	88	85	83	87	84	82	86	84	82
9	93	87	84	81	93	87	83	81	86	83	81	85	83	81	85	82	80
10	91	85	82	79	91	85	82	79	84	81	79	84	81	79	83	81	79

LD6LED4SW35K8SP18

Test No. 2922

NOTES: Refer to www.prescolite.com for additional photometric tests (IES Files).
 1. Operation in ambient temperatures higher than those specified may shorten life and will void warranty.





6- 3/4" Square Up/Down Cylinder

DB6

Two 26W or 32W CFL
Triple Tube 4-Pin Lamp
120V-277V, or 347V

DATE: _____ TYPE: _____

FIRM NAME: _____

PROJECT: _____



For conversion to millimeters,
multiply inches by 25.4
Not to Scale

APPLICATIONS:

The DB6 is a 6-3/4" square wall mounted cylinder used to achieve direct illumination patterns on vertical walls or columns. The DB6 is ideal for a wide variety of commercial, retail, hospitality and institutional applications where the added energy benefit of compact fluorescent sources is required.

HOUSING:

Cylinder is constructed with a heavy-wall, extruded aluminum housing and rugged mounting options. Lens door is cast aluminum and top lens is standard on up/down units. A cast aluminum baffle or prismatic lens is required for bottom.

BALLAST:

Two (2) 26W/32W compact fluorescent Class 'P' multi-watt, multi-volt electronic ballast. High power factor standard.

LAMP:

Two (2) 26W (GX24q-3 base), 32W (GX24q-3 base), or 42W (GX24q-4 base) 4-pin triple tube compact fluorescent lamp. Lamp furnished by others.

SOCKET:

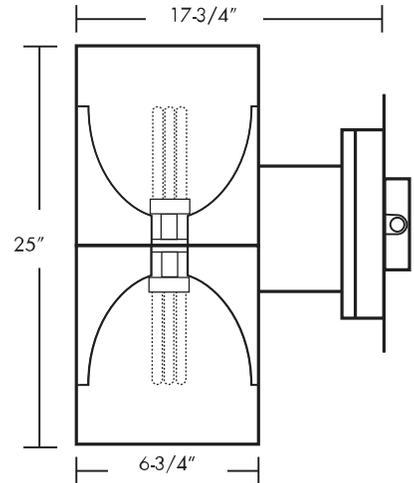
Two (2) injection molded socket. Reflector/socket mounting bracket factory set for specified wattage.

INSTALLATION:

Easy installation onto standard J-box.

LABELS:

CSA/cUS listed for wet locations



CATALOG NUMBER:

EXAMPLE: DB6255PLT32BMW26BL

CYLINDERS	CYLINDER LENGTH	MOUNTING	WATTAGE SOURCE	VOLTAGE	BAFFLE/LENS	OPTIONS	FINISH
<input type="checkbox"/> DB6 6-3/4" Square, compact fluorescent cylinder	<input type="checkbox"/> 25-25"	<input type="checkbox"/> 5' Wall mount, up/down	<input type="checkbox"/> PLT26 26 Watt compact fluorescent <input type="checkbox"/> PLT32 32 Watt compact fluorescent	<input type="checkbox"/> Blank 120-277V <input type="checkbox"/> 347 347V	<input type="checkbox"/> B Baffle <input type="checkbox"/> L Prismatic Glass Lens	<input type="checkbox"/> MW26 Maximum wattage label 26W <input type="checkbox"/> F1 Fusing, 120V <input type="checkbox"/> F4 Fusing, 277V <input type="checkbox"/> F6 Fusing, 347V	<input type="checkbox"/> BZ Bronze <input type="checkbox"/> BL Matte Black <input type="checkbox"/> WH Matte White <input type="checkbox"/> Z Zet

NOTE
1. Up/Down has top lens standard. Must select L or B for bottom.
2. Options installed at factory

PHOTOMETRIC DATA

LiteForms™ - 6 3/4" Wall Mount Cylinder DB6

BALLAST DATA

	26W			32W		
	120V	277V	347V	120V	277V	347V
Total System Watts	28W	28W	38W	35W	35W	42W
Input Current (Amps)	0.23	0.1	0.11	0.29	0.13	0.12
Input Frequency	50/60Hz	50/60Hz	50/60Hz	50/60Hz	50/60Hz	50/60Hz
Power Factor	>97%	>97%	>97%	>97%	>97%	>97%
Ballast Factor	>98%	>98%	>98%	>98%	>98%	>98%
Total Harmonic Distortion	<10%	<10%	<10%	<10%	<10%	<10%
Minimum Starting Temp.	-18°C (0°F)					

LAMP DATA

	26W Triple	32W Triple
Rated Watts	1800	2400
Rated Lumens	1800	2400
Efficacy (LPW)	69	75
Rated Life	10,000 hours	10,000 hours
CRI	82	82
Minimum Starting Temp.	0° F	0° F



KRAUS-ANDERSON BLOCK REDEVELOPMENT

810 Portland Avenue South
Minneapolis, MN 55404

KA BLOCK
REDEVELOPMENT
MINNEAPOLIS, MN



ES&C
ENGINEERING & SURVEYING
ARCHITECTS
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SUITE 1000
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F 612.339.3392
WWW.ESANDC.COM

THIS PLAN OR ANY PART THEREOF IS UNLAWFUL UNLESS IT IS USED IN ACCORDANCE WITH THE TERMS AND CONDITIONS OF THE PROFESSIONAL SERVICES AGREEMENT BETWEEN ES&C AND THE CLIENT.

DATE: 01/29/2016
PROJECT: KA BLOCK REDEVELOPMENT
SHEET: 21.1.1

NOT FOR CONSTRUCTION

LAND USE
APPLICATION
JANUARY 29, 2016

ORIGINAL ISSUE: 01/29/2016

REVISIONS
No. Description Date

2.13.503
PROJECT NUMBER
ES&C
DATE: 01/29/2016

KA BLOCK REDEVELOPMENT

TITLE SHEET
T.1.1

BUILDING AND PARKING DATA

KA BLOCK BUILDING AND PARKING DATA
Site Area: 109,571 SF or 2.515 acres

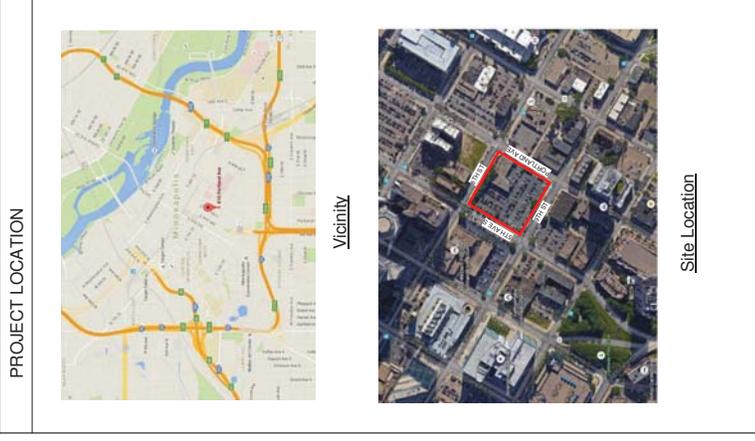
Name	GFA	Stories	Height In Feet	Building Footprint	DUs or Guest Rooms
Apartment Building	293,200	17	184'-8"	21,272	306
Office Building	103,000	5	84'-6"	20,680	NA
Hotel/Brewery Building	158,300	8 and 3	103' and 70'	29,211	161
Total	554,500			71,163	

Underground Parking	Level P1	Level P2	Total
	207,384	2 below	NA
	103,692		NA

Vehicular Parking Data	Total	
	Level P1	Level P2
Standard	157	174
Compact	88	89
Handicap / ADA	6	6
Total	251	269

Tandem Parking	Total	
	Level P1	Level P2
	0	0
	0	0

Bicycle Parking	Total	
	Long Term	Short Term
Apartment Building	306	16
KA Office Building	4	6
Office Innovation		
Hotel		
Hotel Dining/Meeting		
Brewery		
Event Center	12	28
Total	322	40



PROJECT TEAM

General Contractor:
Kraus-Anderson Construction
529 South Eighth St
Minneapolis, MN 55404
chad.rempa@krausanderson.com

Architect:
Elves Swenson Graham Architects Inc.
Contact: Bert Collins, South Site 1080
Minneapolis, MN 55415
bert.collins@esgarch.com

Structural Engineer:
Mayer Brown Johnson
510 Marquette Ave, Suite 800
Minneapolis, MN 55402
Ph: 612.604.8262
mjohnson@mbj.com

MEP:
Sunde Engineering, PLLC
Contact: Luke Kates
1000 W. Washington Ave, Suite 1000
Bloomington, MN 55407
lukem@se-engineers.com

Landscaper:
Damon Fisher Landscape Architects
401 2nd Ave North, Suite 410
Minneapolis, MN 55401
jerry.phillips@damonfisher.com

Available on Request

Mechanical, Electrical, Plumbing Engineers:

SHEET INDEX

Sheet Number	Sheet Name
GENERAL INFORMATION	TITLE SHEET
T.1.1 SURVEY	SURVEY
CIVIL	DEMO/LOCATION PLAN
C1.1	GRADING AND DRAINAGE PLAN
C1.2	NOTES AND DETAILS
C1.3	EROSION CONTROL & STORMWATER
C1.4	LANDSCAPE PRELIMINARY PLAN
C1.5	SCULPTURE SYSTEM DETAILS
C1.6	CIVIL SITE PLAN
C1.7	LANDSCAPE PLAN
L1.0	LANDSCAPE PUBLIC REGULATION PLAN
L1.1	PAVING PLAN
L1.2	LANDSCAPE DETAILS
L1.3	RENDERED LANDSCAPE PLAN
L1.4	ARCHITECTURAL
A0.0	EXISTING SITE IMAGES
A0.1	ARCHITECTURAL SITE PLAN
A0.2	DESIGN PERSPECTIVES
A0.3	DESIGN PERSPECTIVES
A0.4	DESIGN PERSPECTIVES
A0.5	DESIGN PERSPECTIVES
A0.6	DESIGN PERSPECTIVES
A0.7	DESIGN PERSPECTIVES
A0.8	DESIGN PERSPECTIVES
A0.9	SHADOW STUDY
A1.0	LEVEL PLAN
A1.1	STREET LEVEL PLAN
A1.2	LEVEL PLAN
A1.3	LEVEL PLAN
A1.4	LEVEL PLAN
A1.5	LEVEL PLAN
A1.6	LEVELS 10-17 RISE PLANS
A2.0	RESIDENTIAL ELEVATIONS
A2.0a	RESIDENTIAL ELEVATIONS
A2.0b1	RESIDENTIAL ELEVATIONS
A2.0c	RESIDENTIAL ELEVATIONS
A2.1	RESIDENTIAL ELEVATIONS
A2.1a	HOTEL AND BREWERY ELEVATIONS
A2.1b	HOTEL AND BREWERY ELEVATIONS
A2.1c	HOTEL AND BREWERY ELEVATIONS
A2.1d	HOTEL AND BREWERY ELEVATIONS
A2.2	OFFICE ELEVATIONS
A2.2a	OFFICE ELEVATIONS
A2.2b1	OFFICE ELEVATIONS
A2.2b2	OFFICE ELEVATIONS
PLAT	PRELIMINARY PLATS



ESC
ENGINEERING & SURVEYING
CONSULTANTS
INCORPORATED
1111 W. WASHINGTON ST.
CHICAGO, IL 60607
TEL: 312.467.8800
WWW.ESCENGINEERING.COM

I hereby certify that this document was prepared by me or under my direct supervision and that I am a duly Licensed Engineer under the laws of the State of Illinois.

SCALE: AS SHOWN
DATE: 01/29/16
PROJECT: KA BLOCK REDEVELOPMENT
DRAWN BY: [Name]
CHECKED BY: [Name]



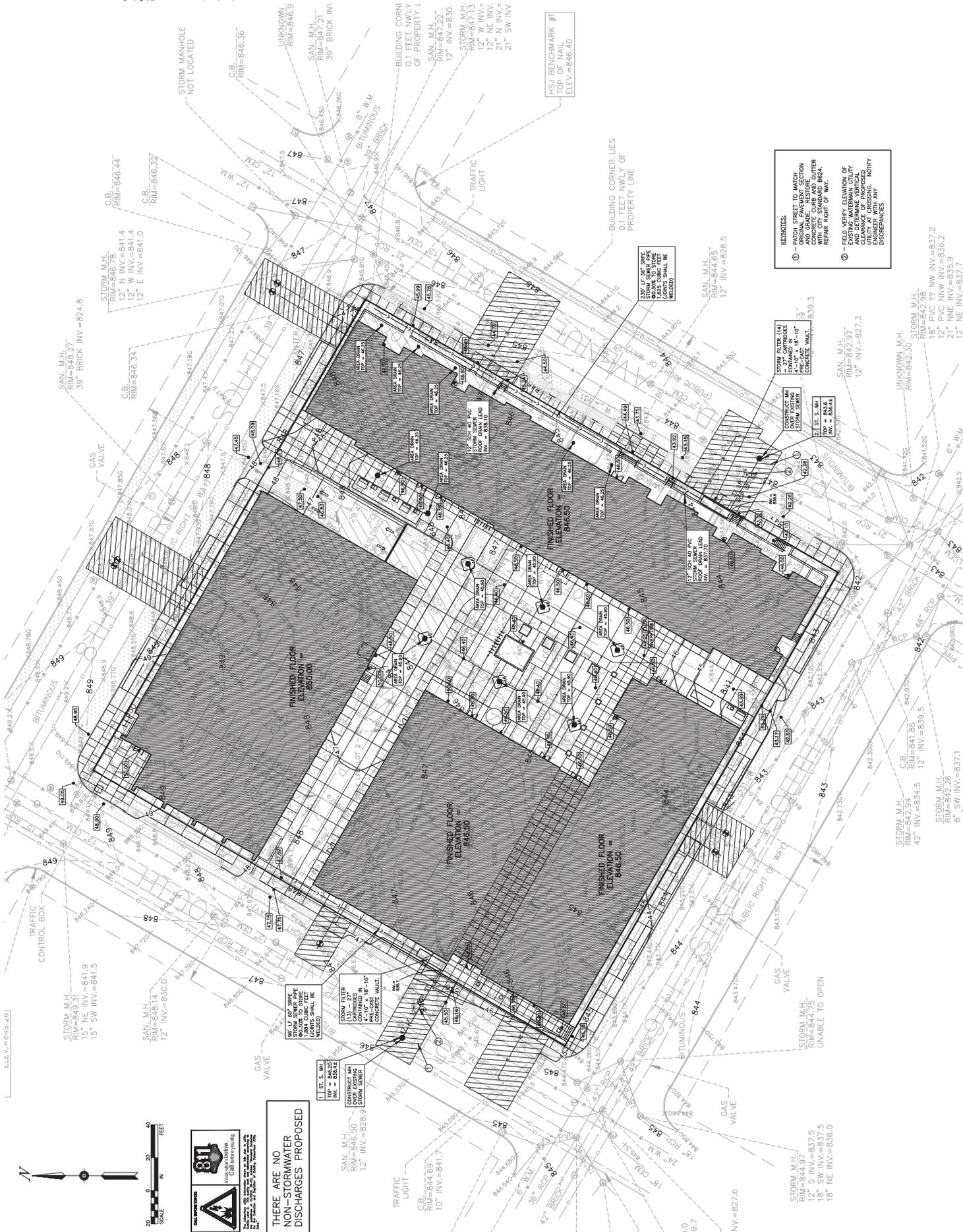
Sunde
ENGINEERING & SURVEYING
INCORPORATED
1111 W. WASHINGTON ST.
CHICAGO, IL 60607
TEL: 312.467.8800
WWW.SUNDEENGINEERING.COM

NOT FOR CONSTRUCTION

LAND USE APPLICATION
1/29/16

REVISIONS	DATE	BY
215503	01/29/16	ESC
215504	01/29/16	ESC

KA BLOCK REDEVELOPMENT
GRADING AND DRAINAGE PLAN
C1.2



NOTES:

- PATCH STREET TO MATCH ORIGINAL PAVEMENT SECTION CONCRETE CURB AND GUTTER WITH EXISTING PAVEMENT TO MATCH RIGHT OF WAY.
- FIELD VERIFY ELEVATION OF EXISTING VERTICAL CURB AND GUTTER AND DETERMINE VERTICAL CLEARANCE OF PROPOSED PAVEMENT WITH ANY DISCREPANCIES.

30% UP 30% SLOPE
BASED ON STORM
WATER FLOW
JOINTS SHALL BE
WELDED

STORM FILTER (14)
CONTAINED IN 10' OF
PRE-CAST
CONCRETE WALL

CONSTRUCT 10' OF
PRE-CAST
CONCRETE WALL
TOP = 845.4
INV. = 837.7

STORM M.H.
RIM=842.94
42" INV.=834.5
12" INV.=839.5

STORM M.H.
RIM=842.26
8" SW INV.=837.1
48" INV.=834.8

STORM M.H.
RIM=844.97
12" S INV.=837.5
18" SW INV.=837.5
18" NE INV.=836.0

THERE ARE NO
NON-STORMWATER
DISCHARGES PROPOSED



ELEV.=049.420

40.0
339.7
15' INV.=827.6



ES&C
ENGINEERS & ARCHITECTS
1100 N. W. 10th Street, Suite 100
Fort Lauderdale, FL 33304
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www.esandc.com

I hereby certify that this document was prepared by me or under my direct supervision and that I am a duly Licensed Engineer under the laws of the State of Florida.

REGISTERED PROFESSIONAL ENGINEER
No. 12547
Mechanical
State of Florida
DATE: 12/15/2011



SUNDE ENGINEERING, LLC
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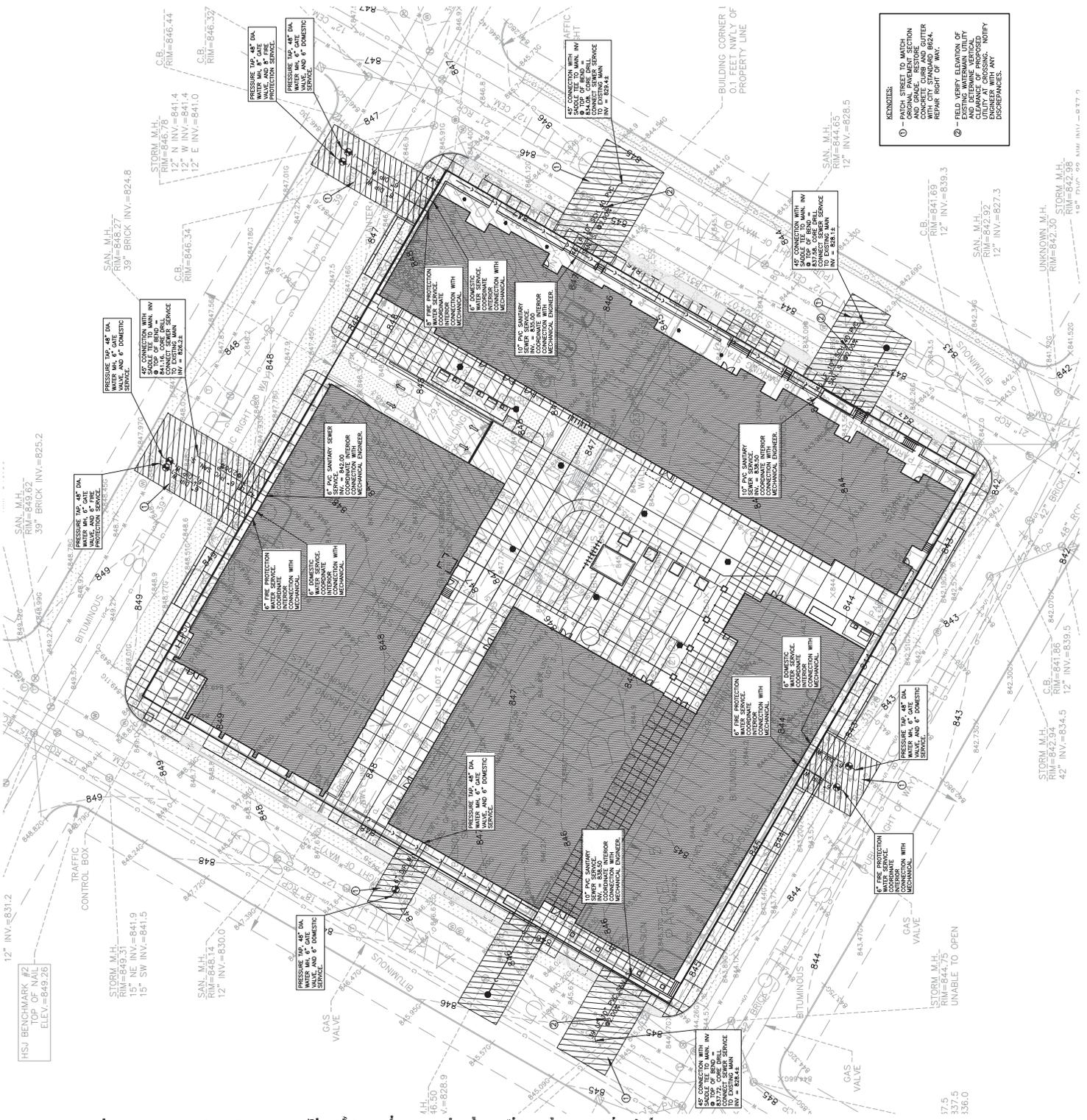
LAND USE APPLICATION
1/29/16

ORIGINAL ISSUE
REVISIONS

215503
PROJECT NUMBER
MK
DRAWN BY
CHECKED BY
DATE

KA BLOCK REDEVELOPMENT
UTILITY PLAN

C1.3



KEYNOTES:
 ① - PATCH STREET TO MATCH CONCRETE CURB AND GUTTER AND GRADE. RESTORE AND REPAIR RIGHT OF WAY.
 ② - FIELD VERIFY ELEVATION OF AND DETERMINE VERTICAL ALIGNMENT OF EXISTING UTILITY AT CROSSING. NOTIFY ENGINEER WITH ANY DISCREPANCIES.

- SANITARY SEWER:**
1. All sanitary sewer lines shall be installed in accordance with the Florida Building Code, Chapter 9, Part 9.05, and the Florida State Plumbing Code, Chapter 9, Part 9.05. All sanitary sewer lines shall be installed in accordance with the Florida Building Code, Chapter 9, Part 9.05, and the Florida State Plumbing Code, Chapter 9, Part 9.05.
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17.5
37.5
336.0



Environmental Science Group
 11111 Hennepin Avenue, Suite 100
 Minneapolis, MN 55424
 Tel: 763.551.1333
 Fax: 763.551.1334
 www.esginc.com

Prepared for: KA Block Redevelopment
 Prepared by: Environmental Science Group
 Date: 1/29/16



Sunde Engineering, Inc.
 11111 Hennepin Avenue, Suite 100
 Minneapolis, MN 55424
 Tel: 763.551.1333
 Fax: 763.551.1334
 www.sunde.com

NOT FOR CONSTRUCTION

LAND USE APPLICATION
 1/29/16

ORIGINAL ISSUE:	
REVISIONS:	No. Description Date
215303	PROJECT NUMBER
1	ISSUE DATE
1/29/16	DATE

KA BLOCK REDEVELOPMENT
 EROSION CONTROL AND
 STORMWATER POLLUTION
 PREVENTION PLAN
C1.5

ITEM	INSTALLATION	CONTROL	DEVICE OPERATION	MAINTENANCE	SCHEDULE	REMOVAL
1. Silt fence	Install prior to any grading or earthmoving activities.	Filter out sediment from runoff.	Inspect weekly for proper operation.	Remove sediment from filter.	After each rain event.	Remove after construction is complete.
2. Silt fence	Install prior to any grading or earthmoving activities.	Filter out sediment from runoff.	Inspect weekly for proper operation.	Remove sediment from filter.	After each rain event.	Remove after construction is complete.
3. Silt fence	Install prior to any grading or earthmoving activities.	Filter out sediment from runoff.	Inspect weekly for proper operation.	Remove sediment from filter.	After each rain event.	Remove after construction is complete.
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14. Silt fence	Install prior to any grading or earthmoving activities.	Filter out sediment from runoff.	Inspect weekly for proper operation.	Remove sediment from filter.	After each rain event.	Remove after construction is complete.
15. Silt fence	Install prior to any grading or earthmoving activities.	Filter out sediment from runoff.	Inspect weekly for proper operation.	Remove sediment from filter.	After each rain event.	Remove after construction is complete.

AGENCY / POSITION	CONTACT PERSON	PHONE NUMBERS
Owner*		
Contractor**		
Erosion Control Installer		
Damon Forger Associates 10000 Hennepin Avenue, Suite 100 Minneapolis, MN 55424 Tel: 763.551.1333 Fax: 763.551.1334	Alexis Symonakis	612-332-7322 office 612-332-7323 cell
Sunde Engineering, Inc. 11111 Hennepin Avenue, Suite 100 Minneapolis, MN 55424 Tel: 763.551.1333 Fax: 763.551.1334	Brain Marwick	(651) 881-3444 office (651) 881-1913 fax

Party responsible for long term operation and maintenance of the permit.

ESTIMATED PRELIMINARY EROSION AND SEDIMENT CONTROL BMP QUANTITIES	ITEM	QUANTITY
Temporary Silt Fence	1	1
Silt Fence	4	4
Erosion Control Blanket	N/A	N/A
Strom Eriqrap	1200	1200 final feet

* BMP quantities are subject to change. Provide BMP quantities to be installed on site to meet or exceed the above quantities.

- MINNEAPOLIS STANDARD EROSION CONTROL NOTES:**
1. All erosion control measures shall be installed prior to any grading or earthmoving activities.
 2. All erosion control measures shall be maintained throughout the construction process.
 3. All erosion control measures shall be removed after construction is complete.
 4. All erosion control measures shall be installed in accordance with the permit conditions.
 5. All erosion control measures shall be installed in accordance with the permit conditions.
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 15. All erosion control measures shall be installed in accordance with the permit conditions.

- INSPECTIONS AND MAINTENANCE REQUIREMENTS:**
1. Inspect all erosion control measures daily for proper operation.
 2. Remove sediment from all erosion control measures after each rain event.
 3. Repair any damaged erosion control measures immediately.
 4. Remove any vegetation that interferes with the operation of erosion control measures.
 5. Maintain all erosion control measures in good working order throughout the construction process.
 6. All erosion control measures shall be installed in accordance with the permit conditions.
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 15. All erosion control measures shall be installed in accordance with the permit conditions.

- SOIL STABILIZATION:**
1. Apply soil stabilizer to all exposed soil surfaces.
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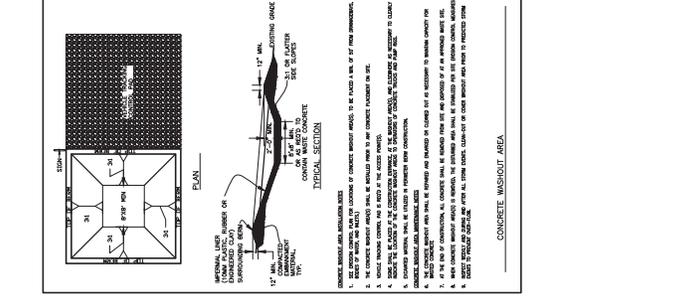
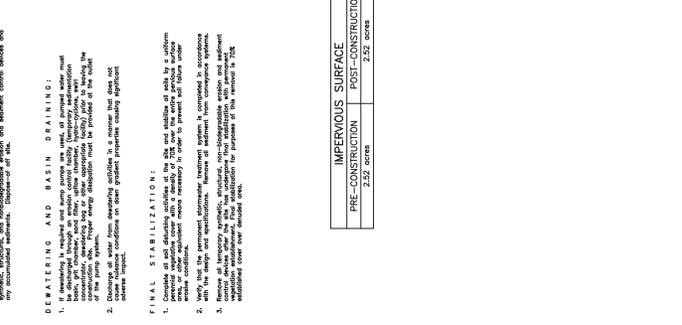
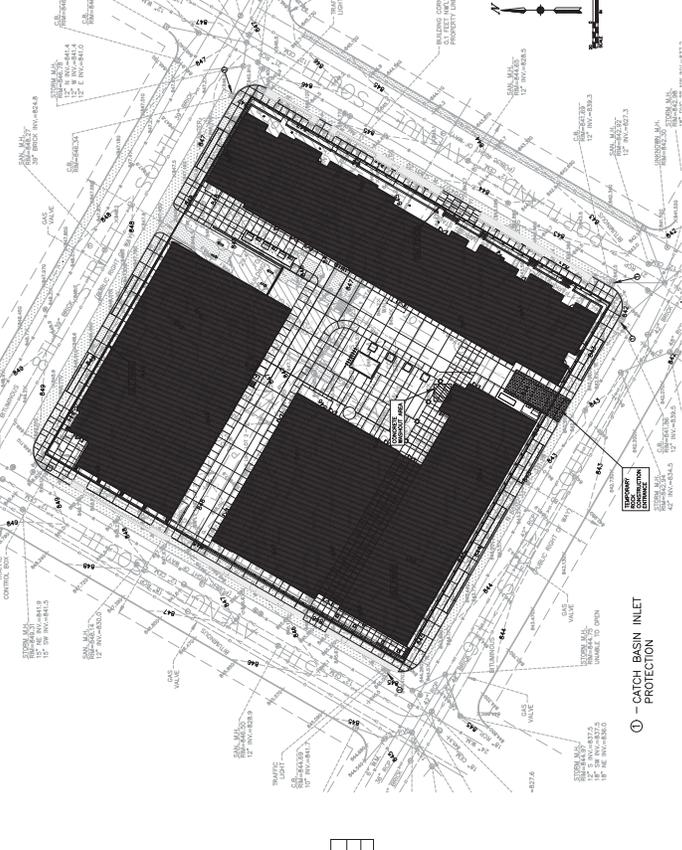
- GENERAL REQUIREMENTS:**
1. All construction activities shall be in accordance with the permit conditions.
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- DEWATERING AND BASIN DRAINAGE:**
1. Install dewatering system to maintain groundwater levels below the foundation.
 2. Install dewatering system to maintain groundwater levels below the foundation.
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ES&C ENGINEERS & ARCHITECTS
 1100 N. CENTRAL AVENUE, SUITE 100
 MINNEAPOLIS, MN 55401
 TEL: 612.338.1100
 FAX: 612.338.1101
 WWW.ESANDARCHITECTS.COM

DATE: 1/29/16
 PROJECT: KA BLOCK REDEVELOPMENT
 DRAWING: STORMWATER DETAIL

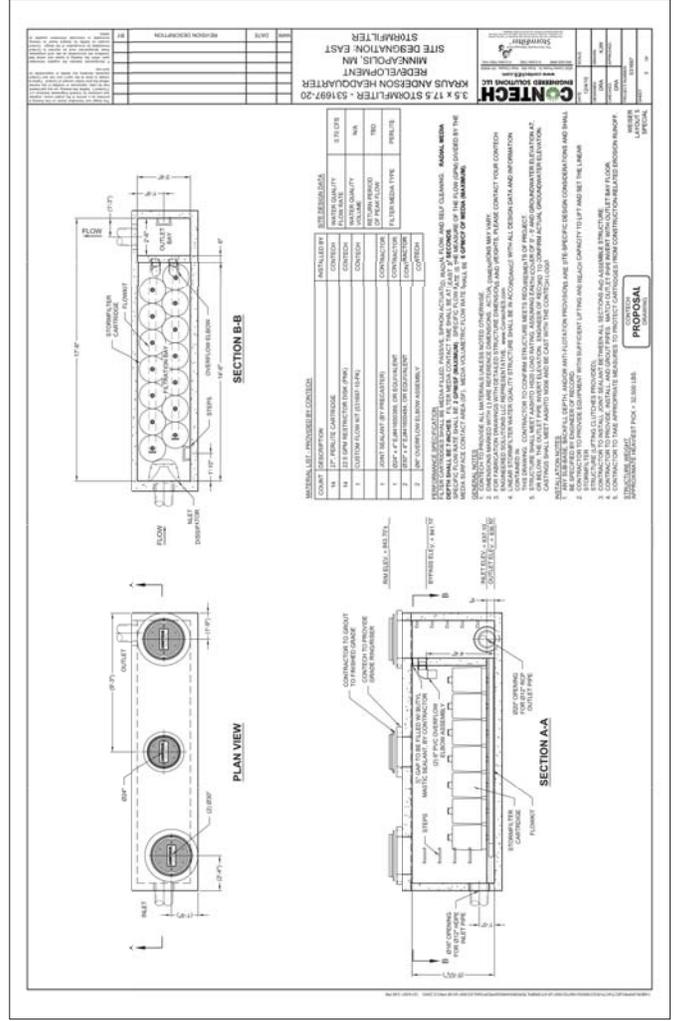
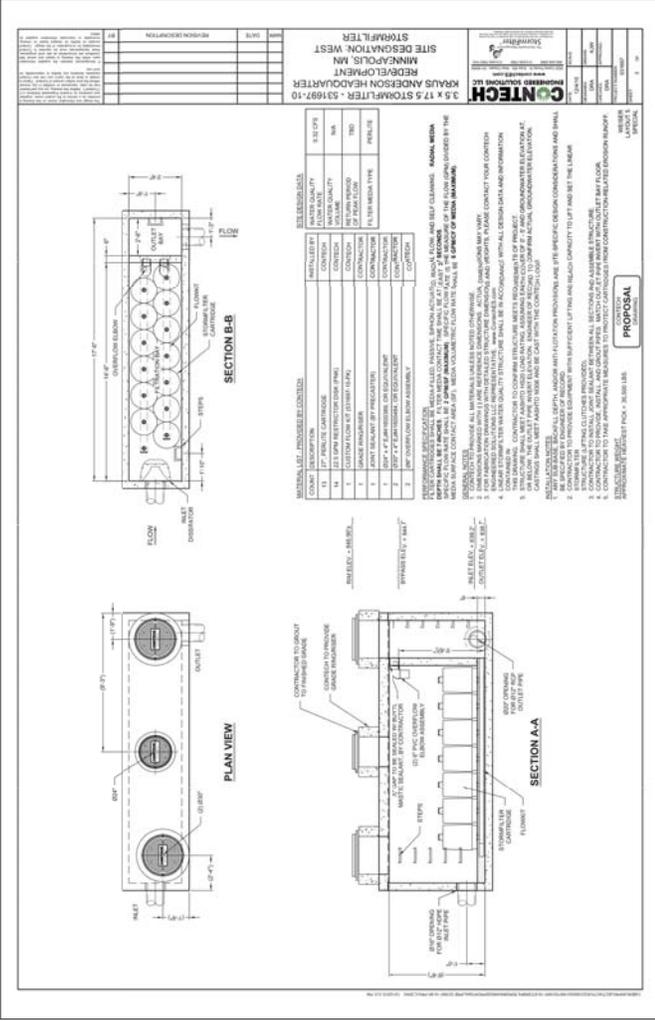


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NOT FOR CONSTRUCTION

ORIGINAL ISSUE
 REVISIONS
 No. Description Date

2/15/03
 PROJECT NUMBER
 MK
 DRAWING NO.
 107-BA



KA BLOCK REDEVELOPMENT

- LEGEND**
- COLOR A - BEIGE - LIGHT SANDBLAST
 - COLOR B - CHARCOAL - LIGHT SANDBLAST
 - COLOR B - CHARCOAL - HEAVY SANDBLAST
 - COLOR C - BROWN - LIGHT SANDBLAST
 - COLOR C - BROWN - HEAVY SANDBLAST
 - CONCRETE PAVERS
 - PROPERTY LINE

esc
 1500 Hennepin Avenue, Suite 110
 Minneapolis, MN 55415
 Tel: 612.339.5382
 www.esgarch.com

I hereby certify that this document was prepared by me or under my direct supervision and that I am a duly licensed landscape architect under the laws of the State of Minnesota.

DATE: _____
 TYPE OF PROJECT: _____
 COUNTY: _____

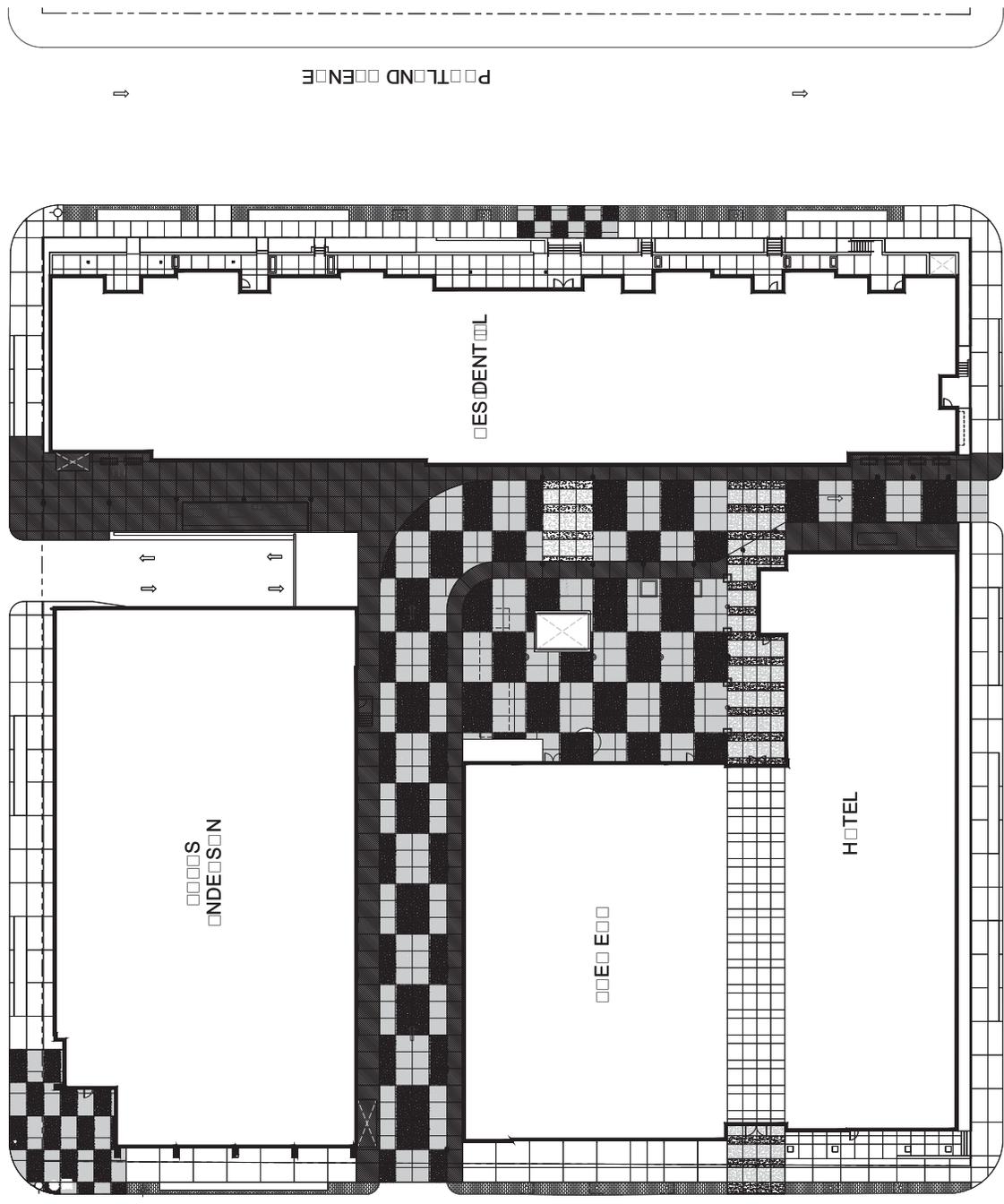
DF/
 DANON FRANK
 LANDSCAPE ARCHITECTS
 411 1st Avenue, Suite 110
 Brooklyn, MN 55401
 PH: 763.332.7922

NOT FOR CONSTRUCTION

LAND USE APPLICATION
 1/29/2016

ORIGINAL ISSUE	REVISIONS	DATE
No.	Description	
215503	ISSUE NUMBER	
1	DATE	
1	PROJECT	
	NO. _____	
	DATE _____	

KA BLOCK REDEVELOPMENT
 PAVING PLAN
 L1.2



TARGET STATION



EXAMPLES OF COLORED CONCRETE WITH SANDBLAST FINISH

COLORED CONCRETE PAVING AREA: 26,174 SF (77% OF TOTAL SITE PAVING)
 REGULAR PAVING AREA: 7,966 SF (23% OF TOTAL SITE PAVING)

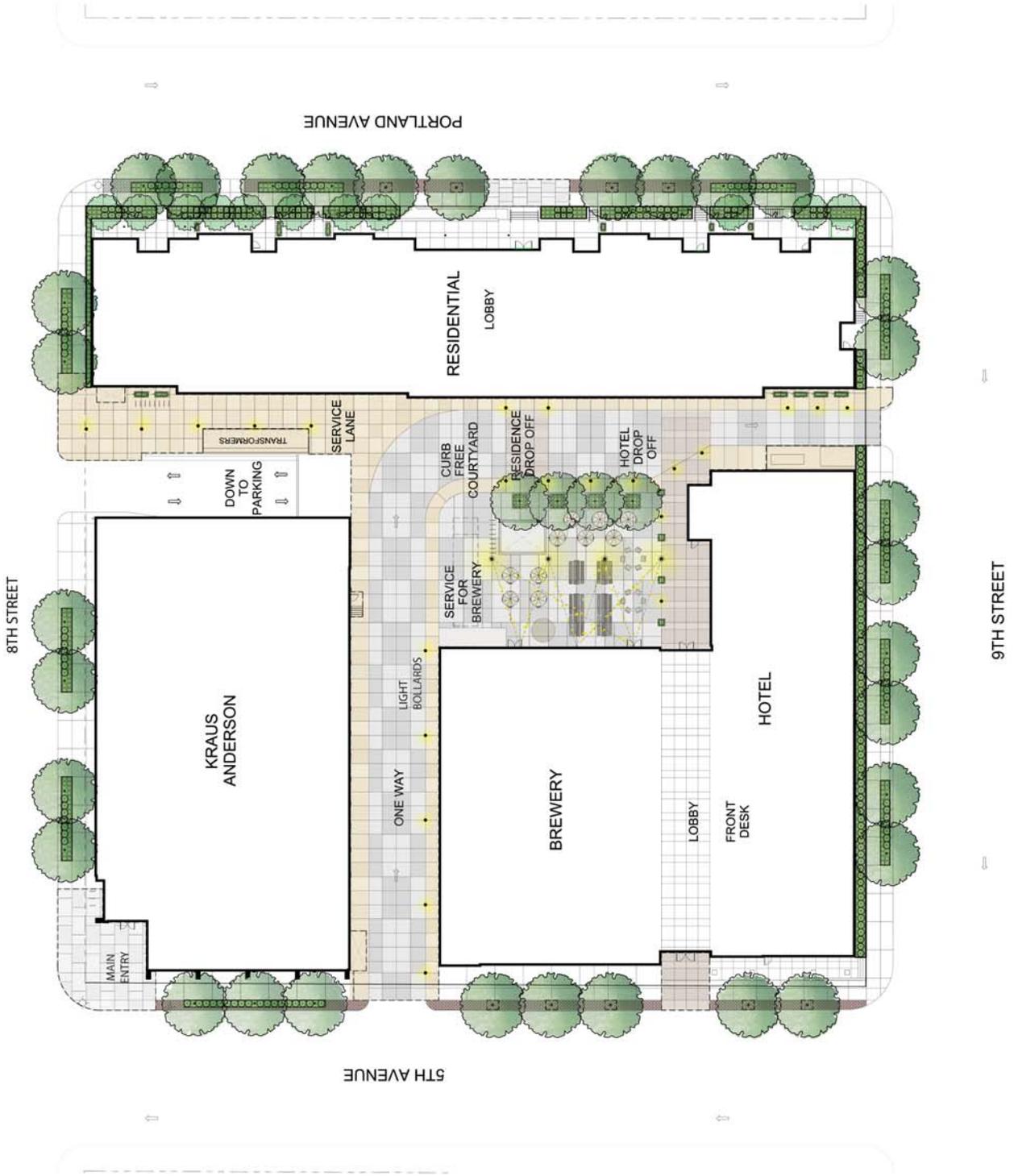
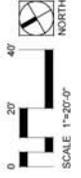


1 PAVING PLAN
 L1.2
 SCALE: 1" = 8'-0"



DAMON FARBER
LANDSCAPE ARCHITECTS

401 2nd Avenue North, Suite 410
Minneapolis, MN 55401
p 612.332.7522
2.16.2016





ES&C INVESTMENT GROUP INC. PROJECT
1000 WASHINGTON AVENUE, SUITE 1000
MINNEAPOLIS, MINNESOTA 55401
TEL: 612.339.5392
WWW.ES&CARCH.COM

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DATE: 01/29/16
PROJECT: KA BLOCK REDEVELOPMENT
DRAWN BY: [Name]

NOT FOR CONSTRUCTION

LAND USE APPLICATION
JANUARY 29, 2016

ORIGINAL ISSUE: 01/29/16

REVISED:

No. Description Date

2.15.2016 Project Update

ES&C

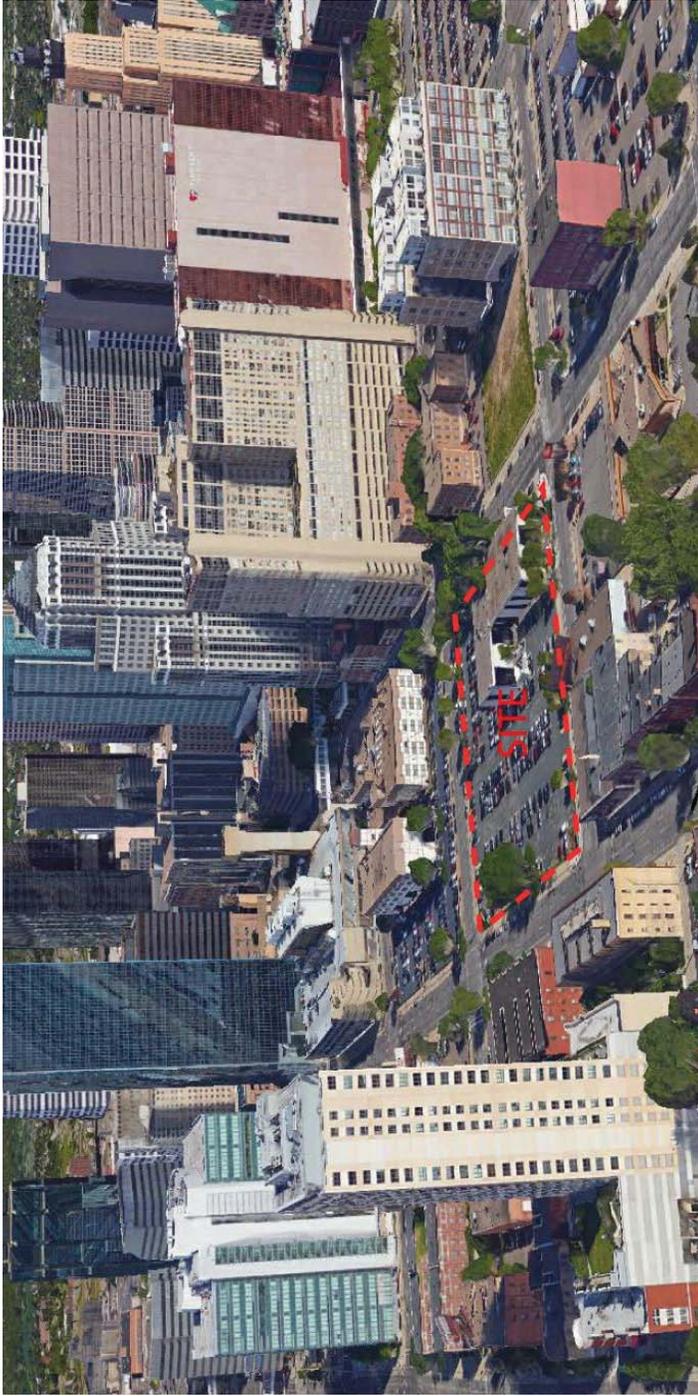
DATE: 01/29/16

BY: [Name]

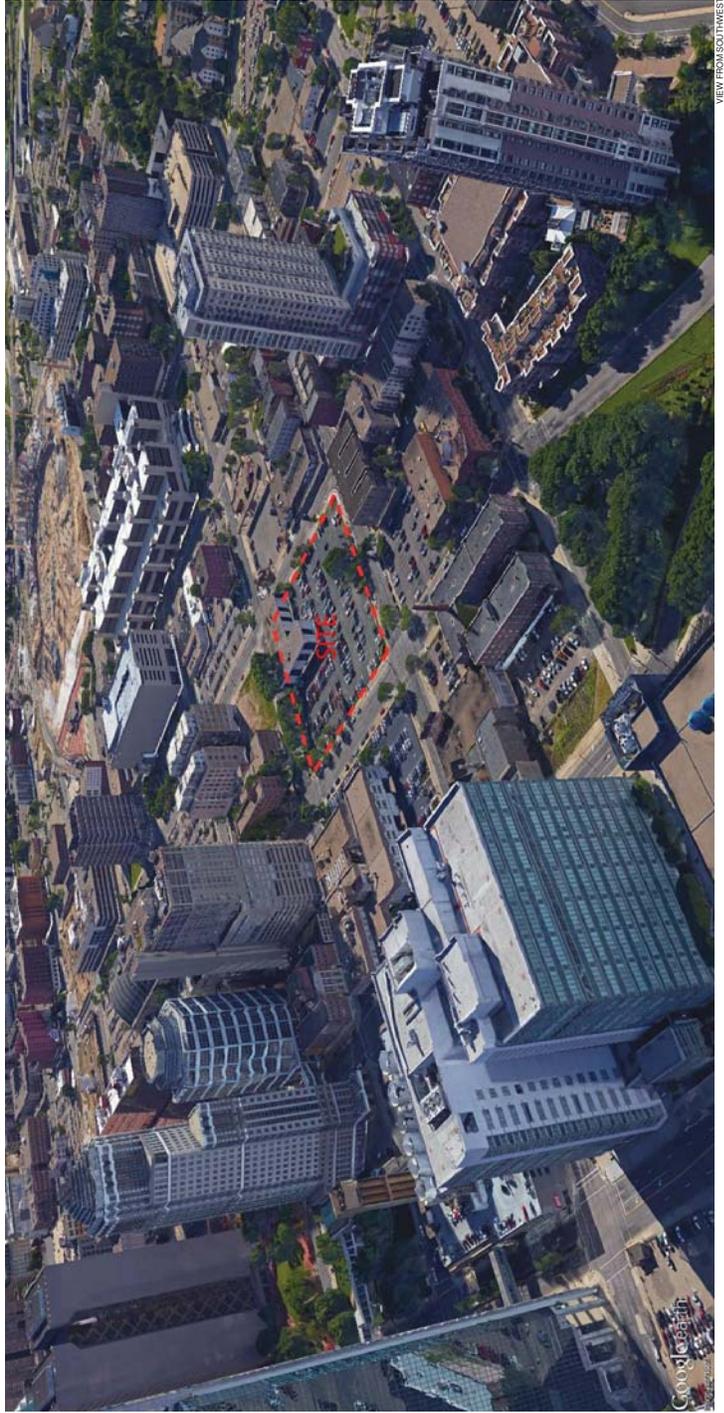
PROJECT: KA BLOCK REDEVELOPMENT

EXISTING SITE IMAGES

A.0.0



VIEW FROM SOUTHEAST



VIEW FROM SOUTHWEST



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LAND USE
APPLICATION
JANUARY 29, 2016

ORIGINAL ISSUE: 01/29/16

REVISIONS

No. Description Date

2/15/2016
Project Update
ES&C
02/15/2016

REVISION

KA BLOCK REDEVELOPMENT

DESIGN PERSPECTIVES
A.0.2



GENERAL VIEW OF DEVELOPMENT

**KA BLOCK
REDEVELOPMENT**
MINNEAPOLIS, MN



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VIEW LOOKING NORTHWEST FROM PLYMOUTH AVENUE



VIEW LOOKING NORTHWEST AT THE INTERSECTION OF PLYMOUTH & 4TH AVENUE

**LAND USE
APPLICATION
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01

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215302

Project Number

Author

Checker

DATE

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KA BLOCK REDEVELOPMENT

DESIGN PERSPECTIVES

A.0.3



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KA BLOCK REDEVELOPMENT

DESIGN PERSPECTIVES

A.0.4



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VIEW OF PORTLAND AVENUE STREETSCAPE



VIEW OF RESIDENTIAL BUILDING FROM HALLWAY COURTYARD



VIEW OF DOWNTOWN MINNEAPOLIS BUILDING ADJACENT TO COURTYARD



VIEW OF DOWNTOWN MINNEAPOLIS BUILDING ADJACENT TO COURTYARD

DETAIL VIEW OF PORTLAND AVENUE STREETSCAPE



LAND USE
APPLICATION
JANUARY 29, 2016

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DESIGN PERSPECTIVES

A.0.5



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01/29/16

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DESIGN PERSPECTIVES

A.0.6



VIEW OF HOTEL & BREWERY FROM AN AERIAL LOOKING SOUTHEAST



VIEW OF HOTEL BUILDING FROM THE STREET LOOKING NORTH



DETAIL VIEW OF HOTEL & BREWERY AND 9TH AVENUE STREETSCAPE



VIEW OF STREETSCAPE



VIEW OF HOTEL & BREWERY ENTRY AT 9TH AVENUE



DETAIL VIEW OF 9TH ST. STREETSCAPE



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PROJECT

KA BLOCK REDEVELOPMENT
DESIGN PERSPECTIVES
A.0.7



NIGHTTIME VIEW OF MULTI-PURPOSE COURTYARD AND BREWERY BUILDING



AERIAL VIEW OF MULTI-PURPOSE COURTYARD LOOKING SOUTHWEST



AERIAL VIEW OF MULTI-PURPOSE COURTYARD LOOKING NORTHWEST



GROUND LEVEL VIEW OF MULTI-PURPOSE COURTYARD FROM HOTEL



GROUND LEVEL VIEW OF MULTI-PURPOSE COURTYARD LOOKING NORTHWEST



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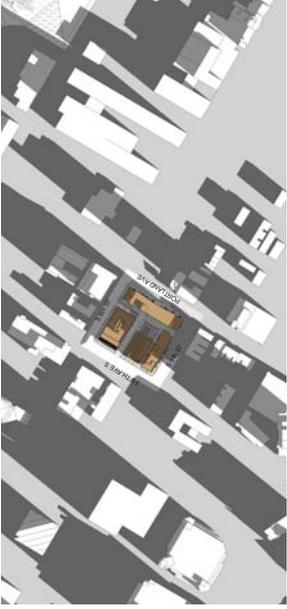
SHADOW STUDY
A.0.9



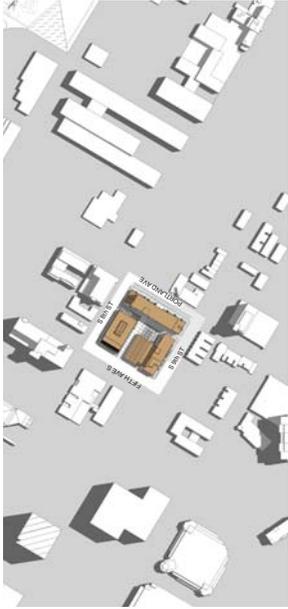
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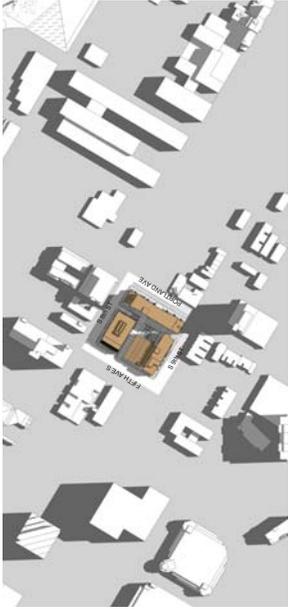
MARCH 10:00AM 10M



DECEMBER 12PM



JUNE 10AM



MARCH 10:00AM 10M



DECEMBER 12PM



JUNE 10AM



MARCH 10:00AM 10M



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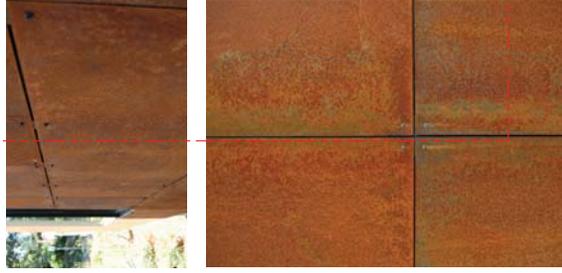
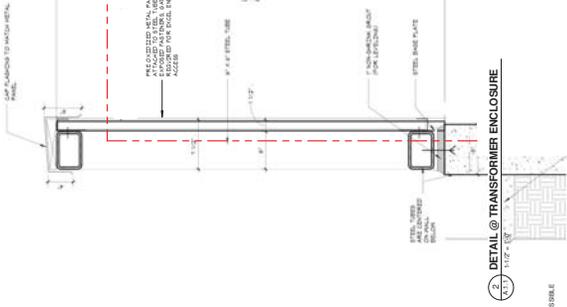
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PROJECT NUMBER
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DATE

KA BLOCK REDEVELOPMENT

STREET LEVEL PLAN
A.1.1



PROVIDED STEEL SCREENING MATERIAL AT BOTH
TYPICAL ENCLOSURES



STREET LEVEL PLAN
117



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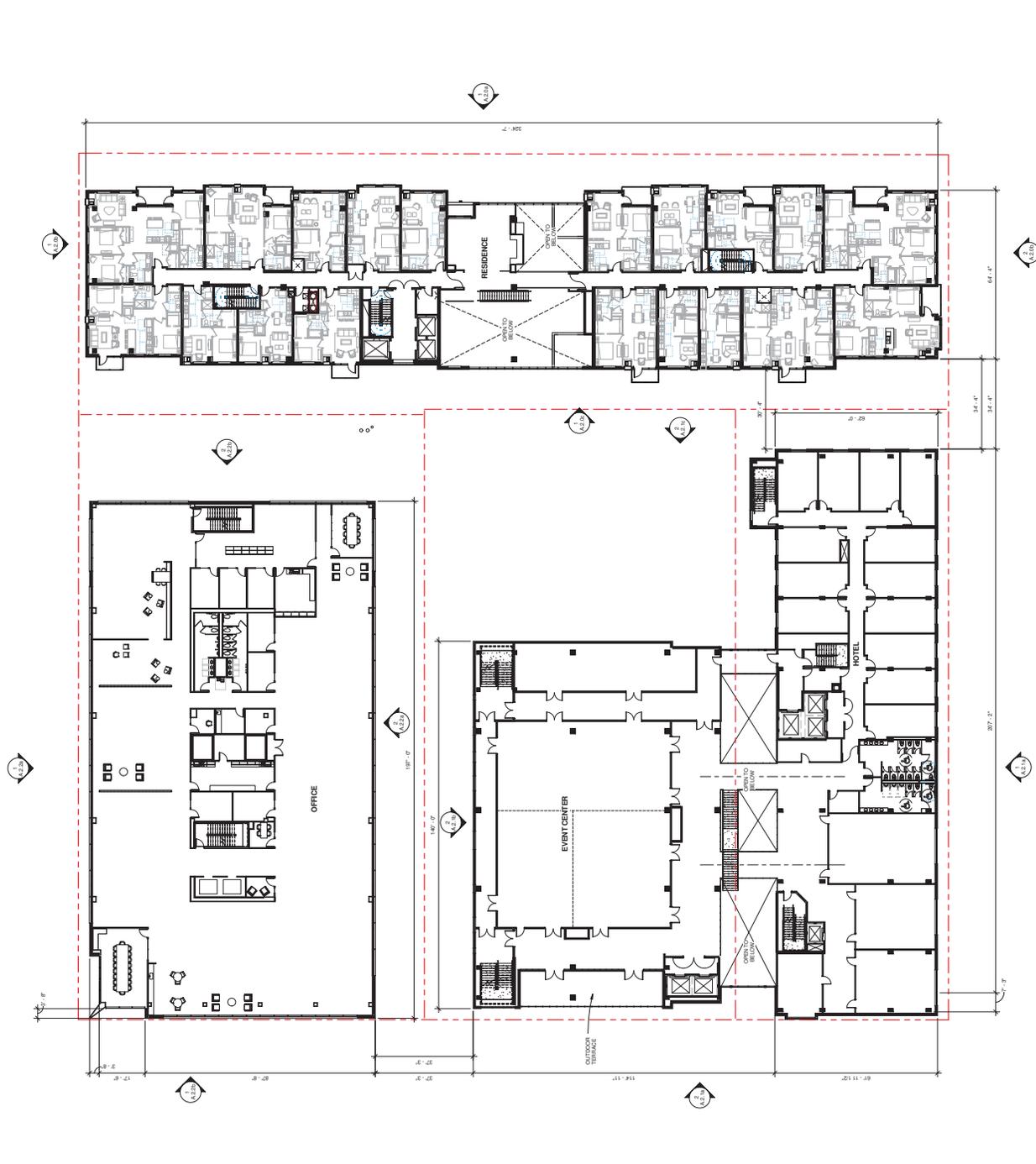
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LEVEL 2 PLAN
A.1.2



LEVEL 2 PLAN
1/29/16



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LEVEL 3 PLAN
A.1.3



LEVEL 3 PLAN
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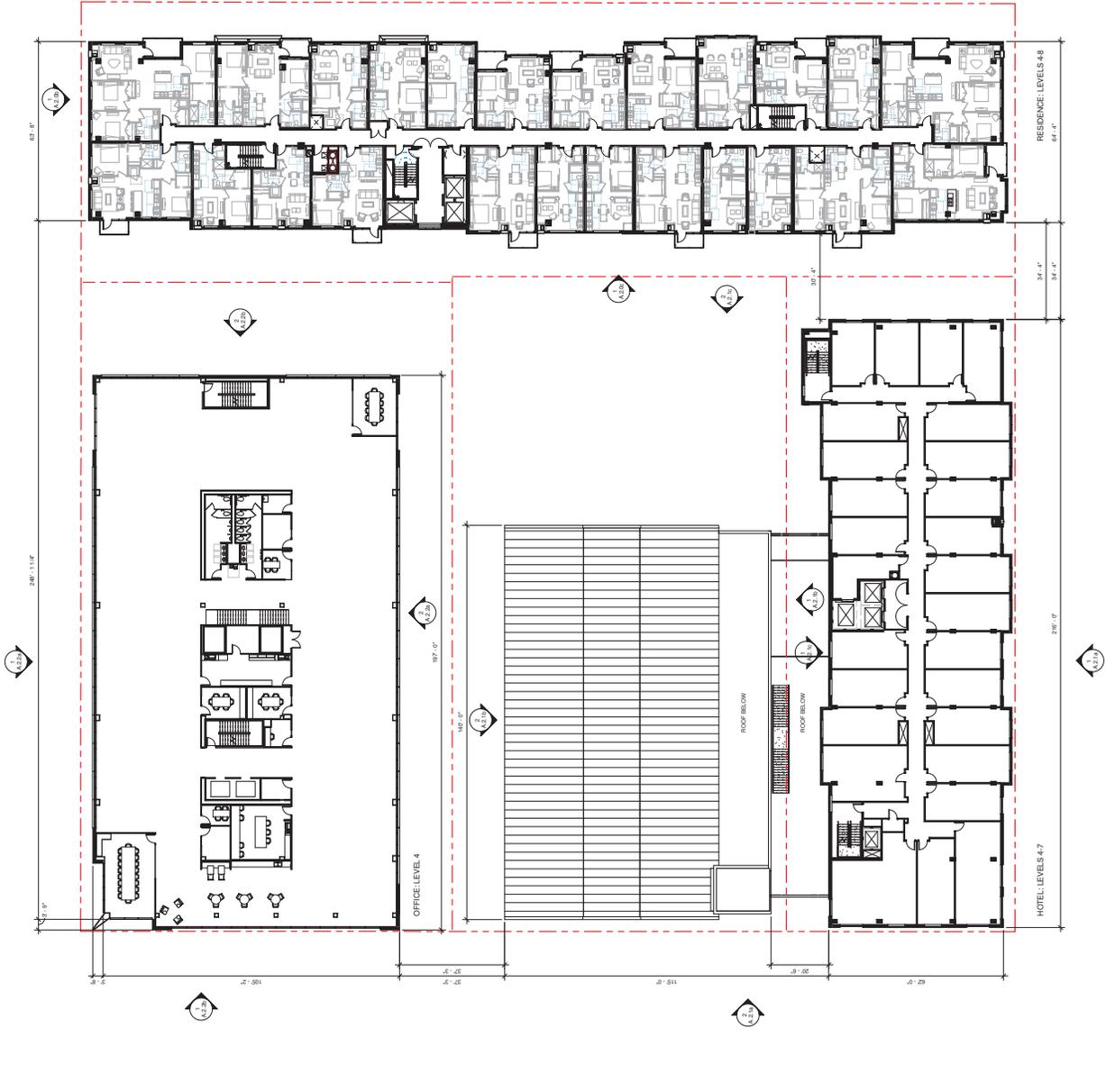
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LEVELS 4-8 PLAN
A.1.4



A.1.1 LEVELS 4-8 PLAN
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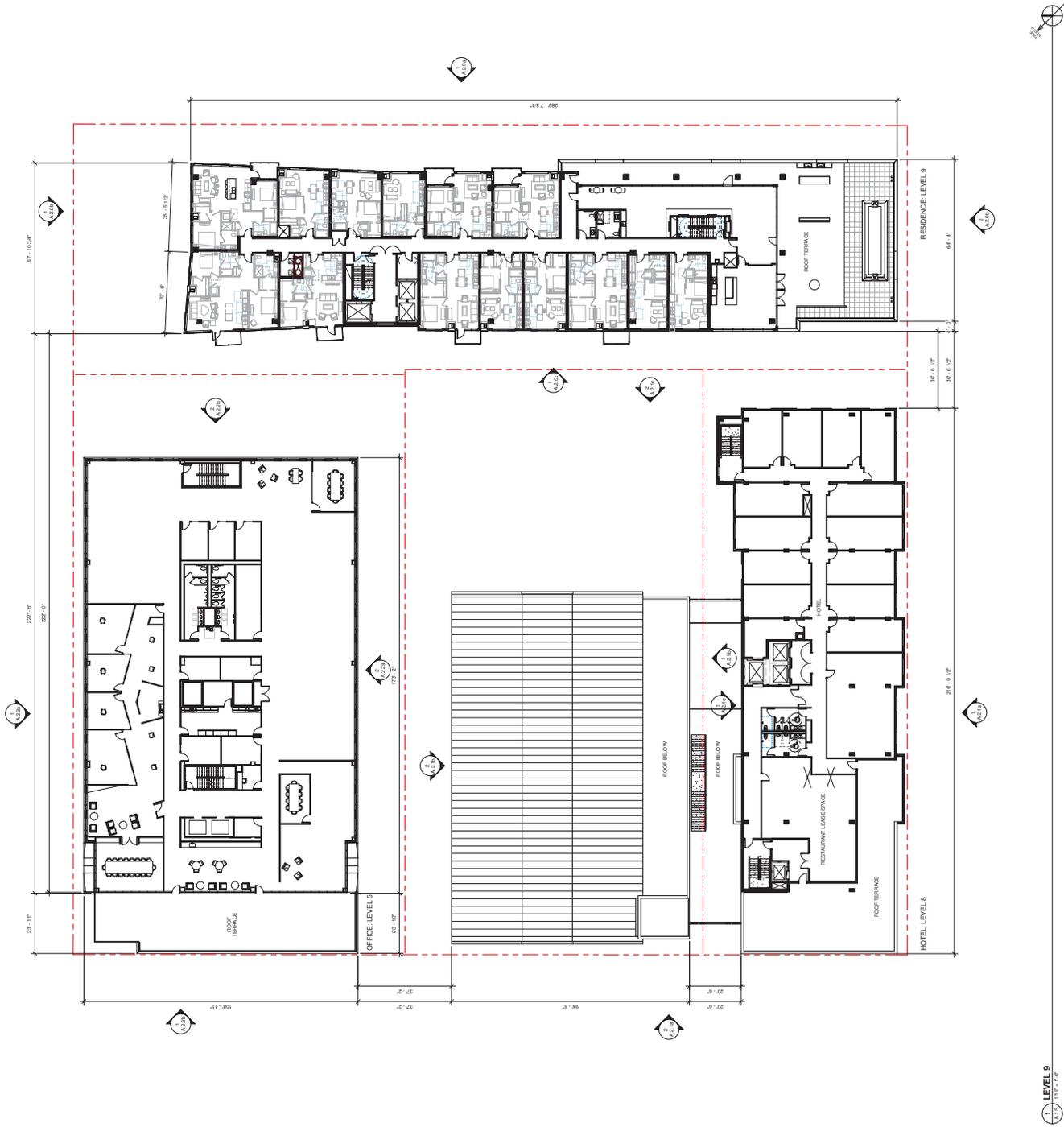
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LEVEL 9 PLAN
A.1.5



LEVEL 9
1/29/16



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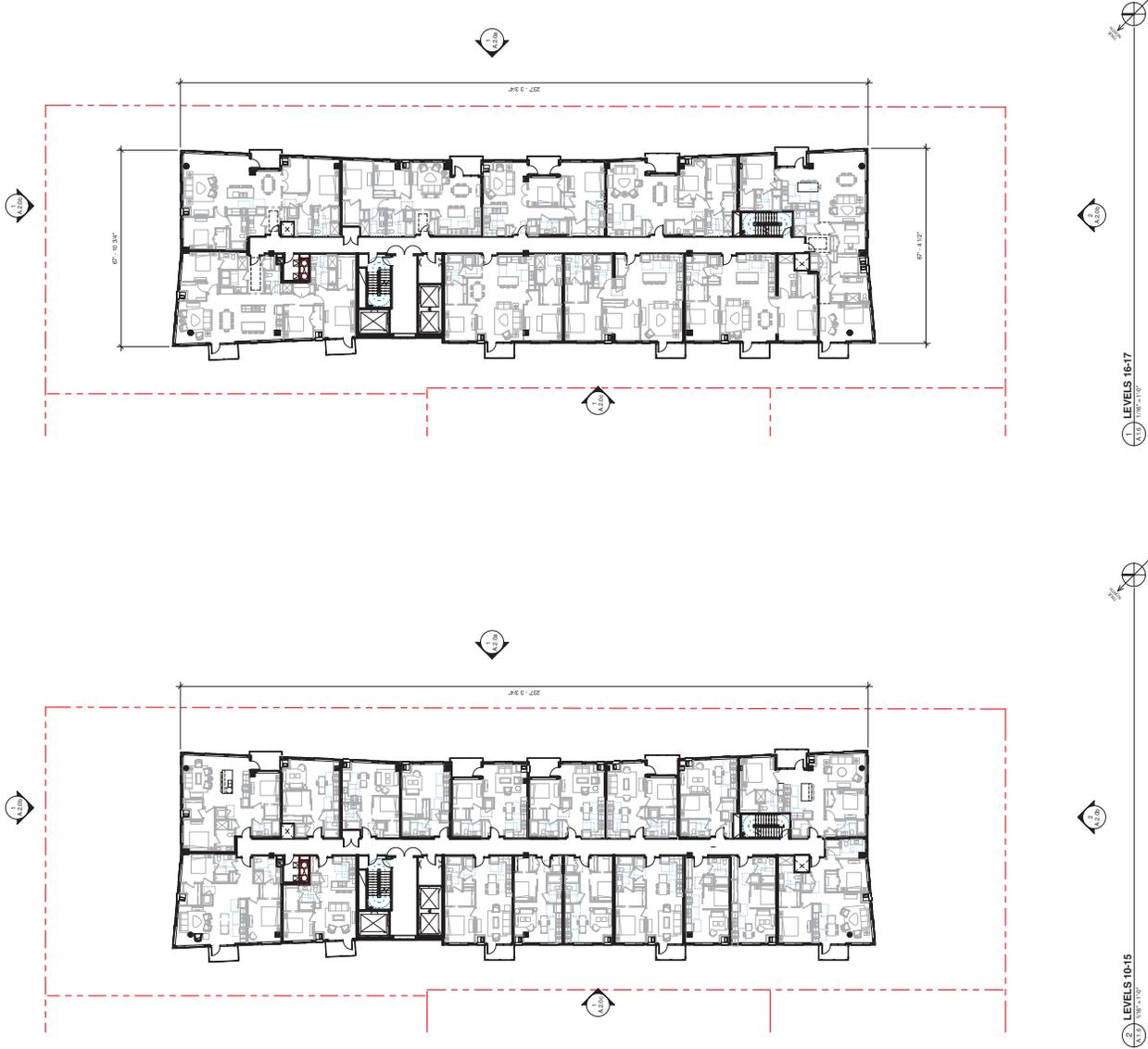
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LEVELS 10-17 RISE PLANS

A.1.6





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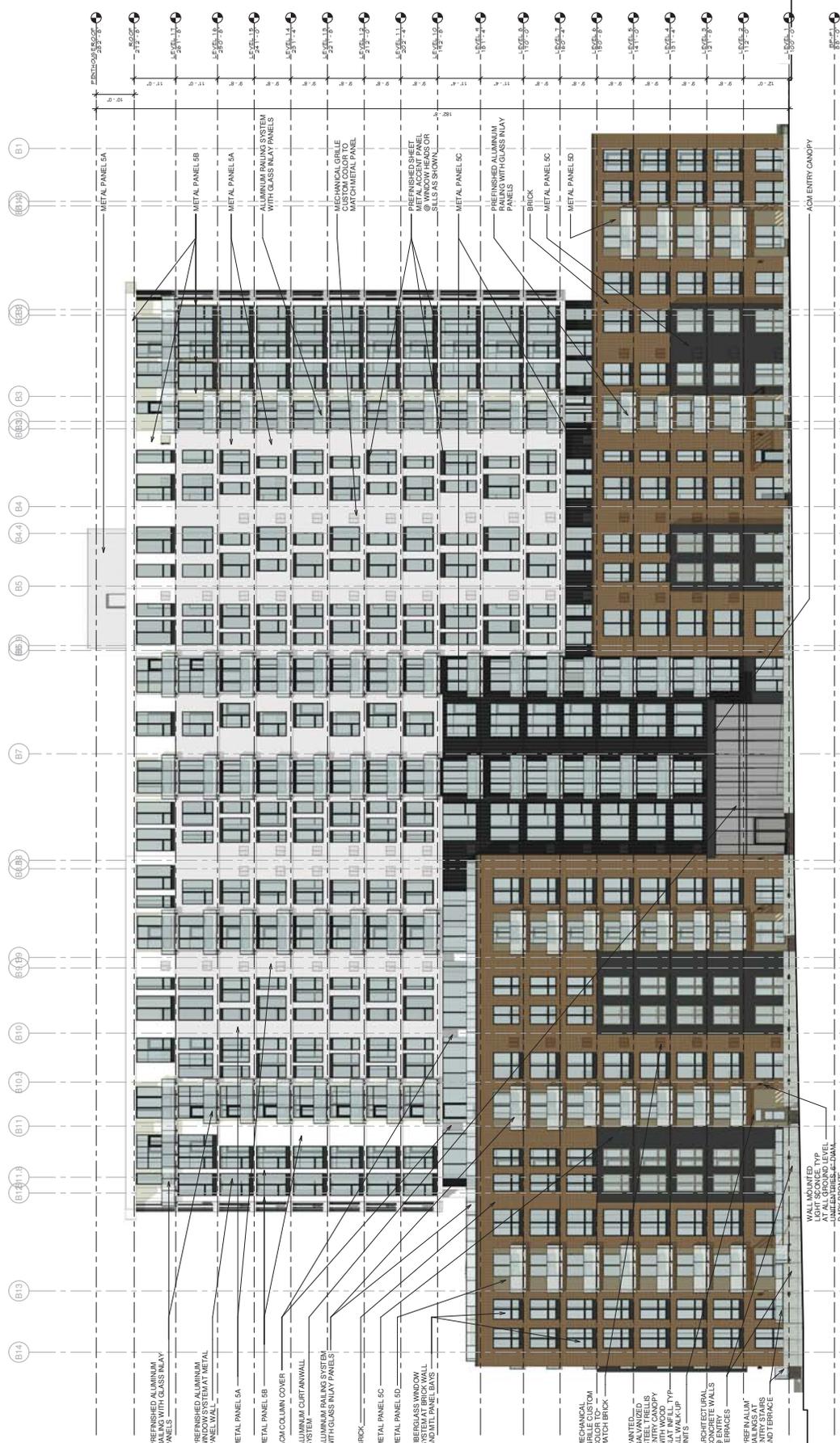
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RESIDENTIAL ELEVATIONS

A.2.0a



RESIDENTIAL - EAST ELEVATION



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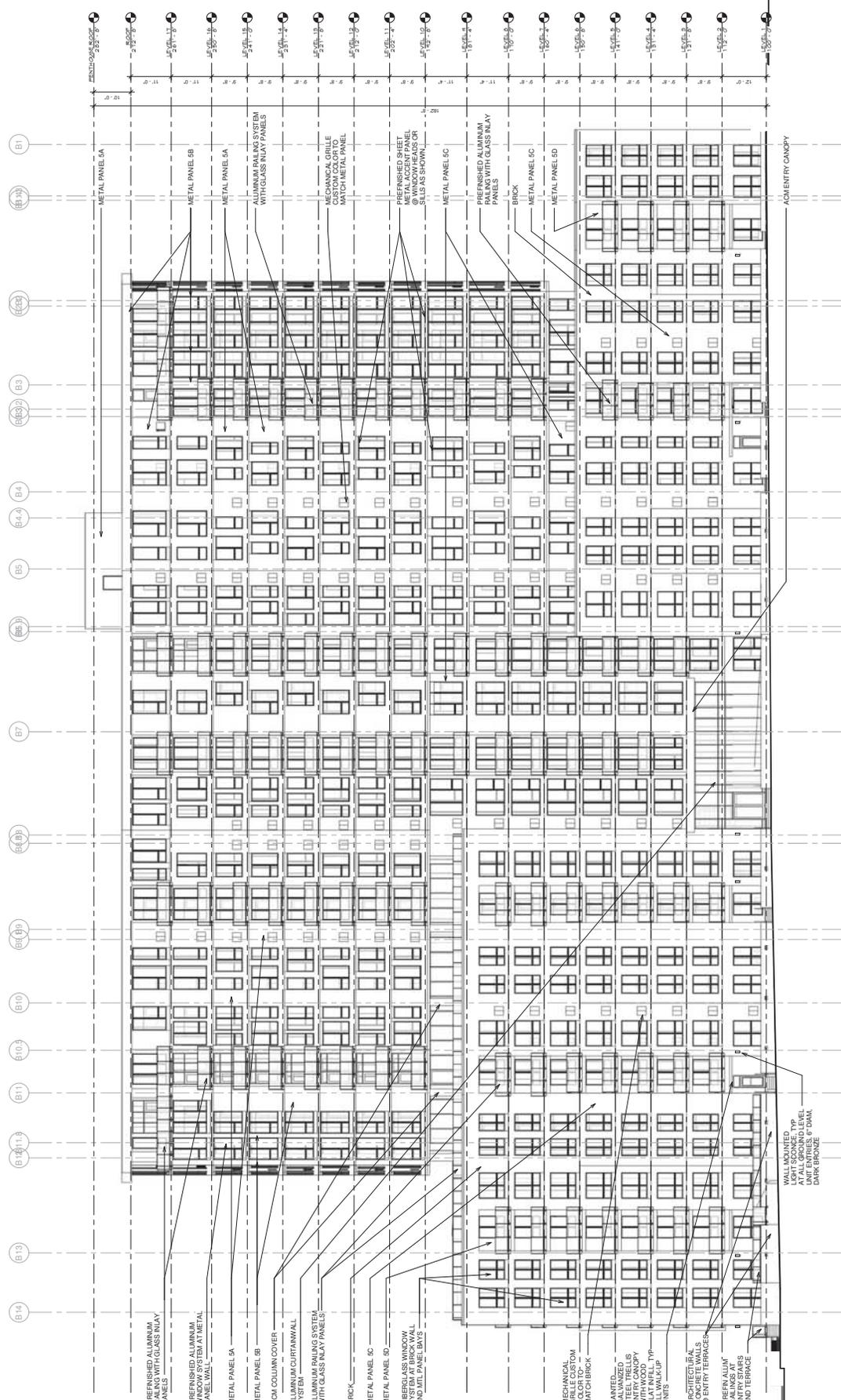
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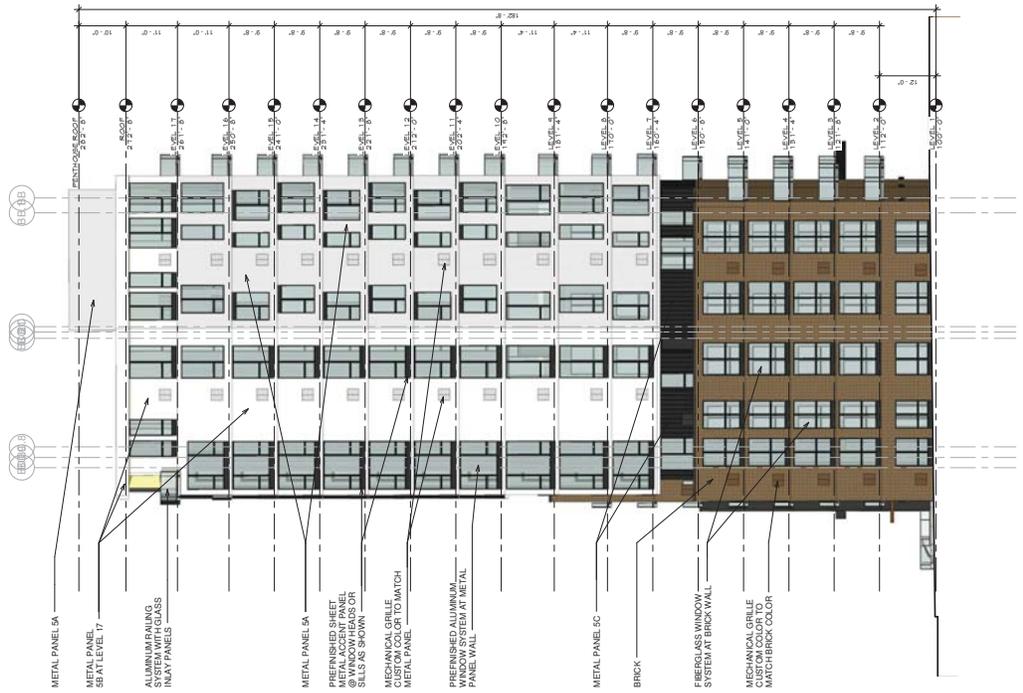
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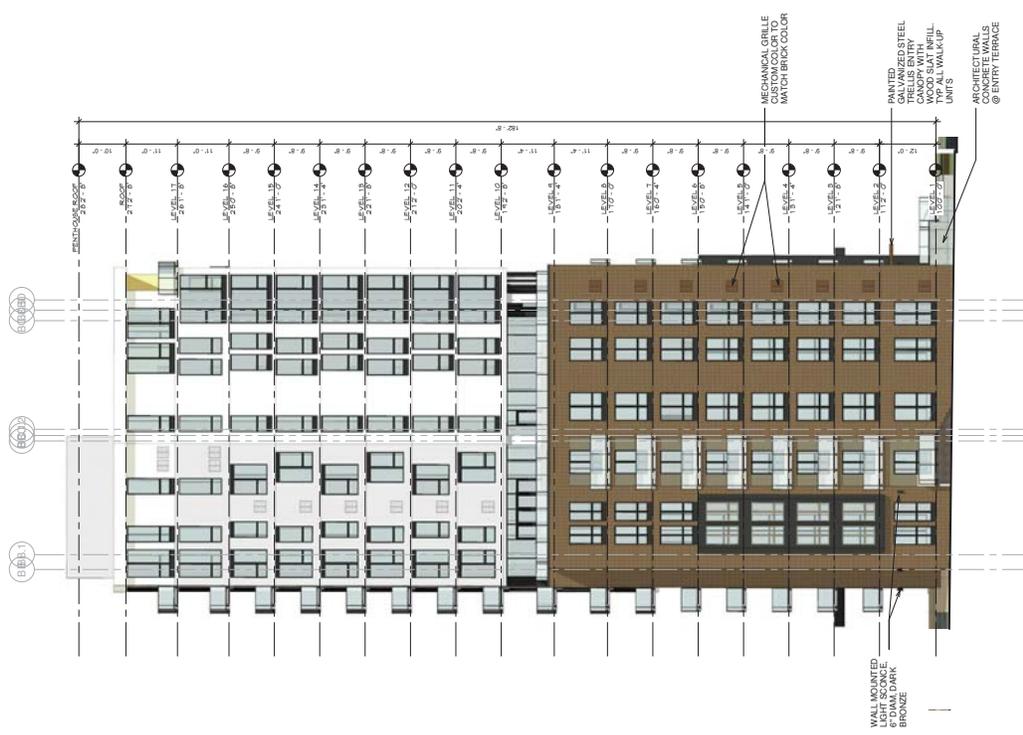
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A.2.0b



1 RESIDENTIAL - NORTH ELEVATION
1/29/16



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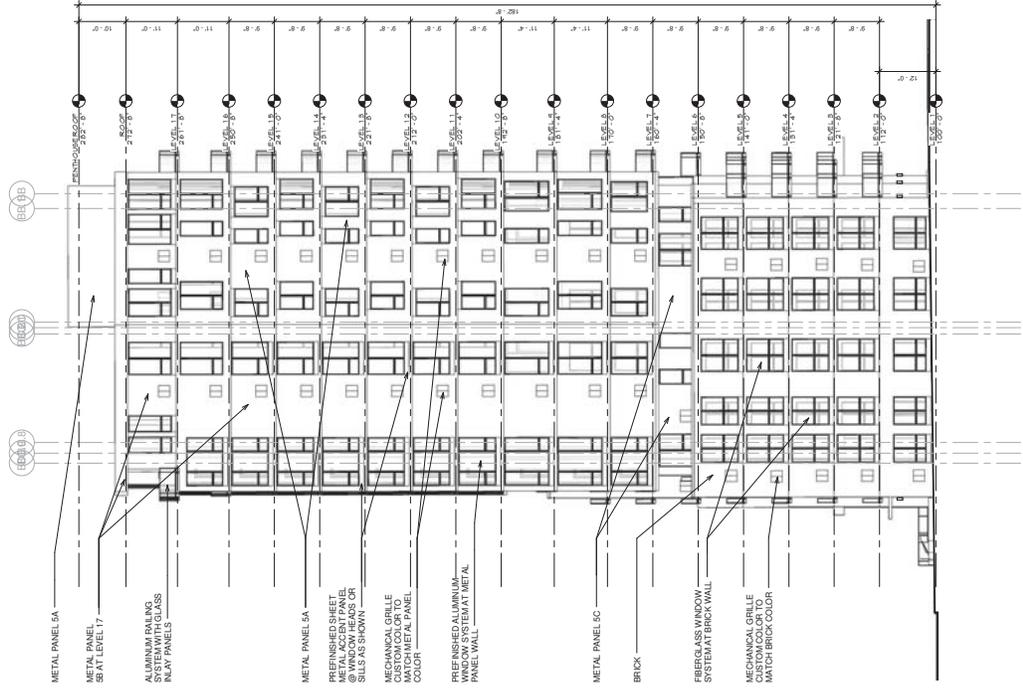
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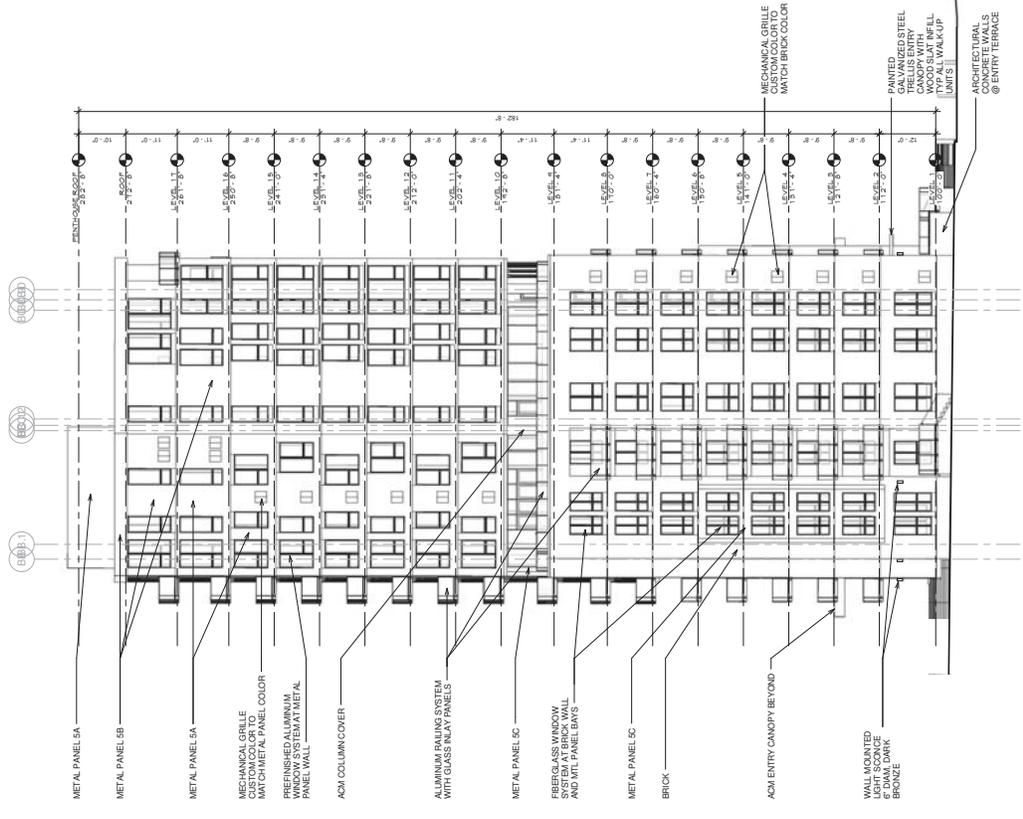
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2.15.002



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2.15.002



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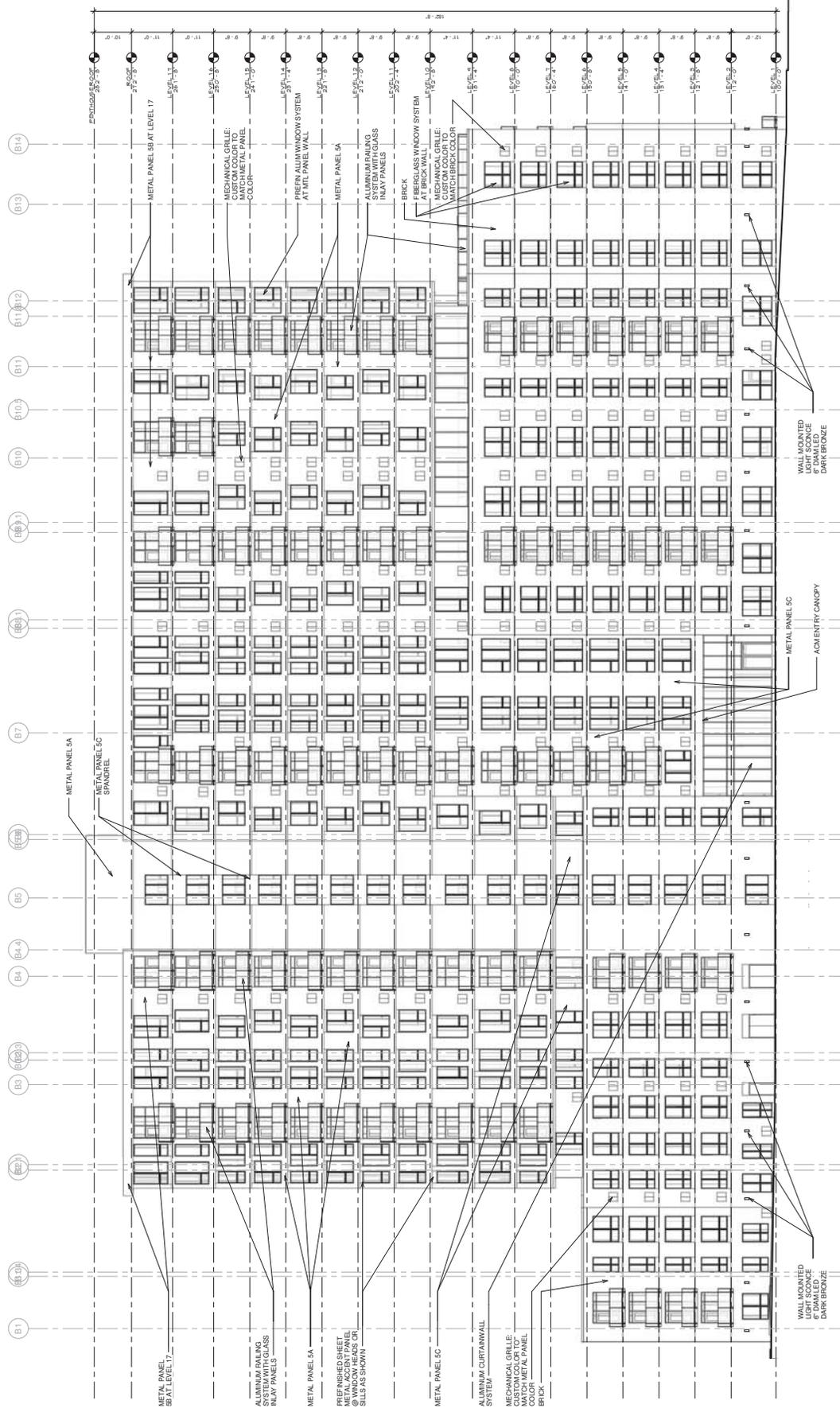
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2.13.502

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Author

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Escalator

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HOTEL AND BREWERY ELEVATIONS

A.2.1a



1. HOTEL - SOUTH ELEVATION



2. BREWERY - WEST ELEVATION



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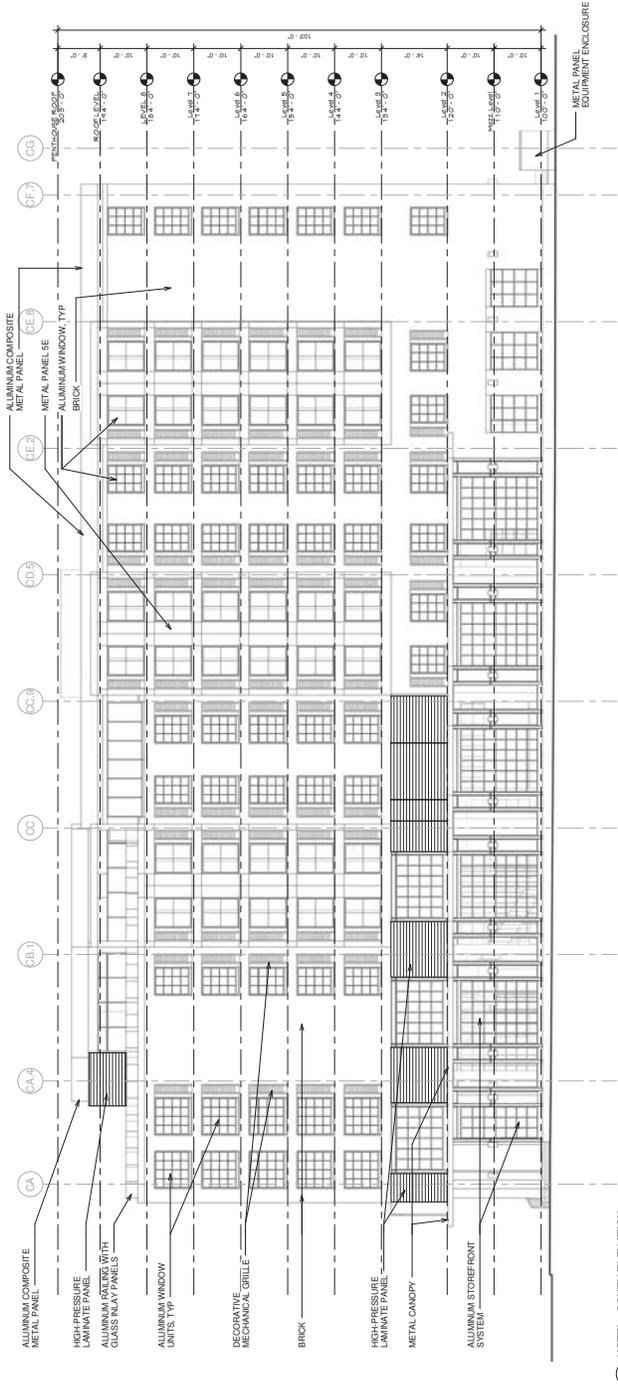
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PROJECT NUMBER
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DATE: [Redacted]

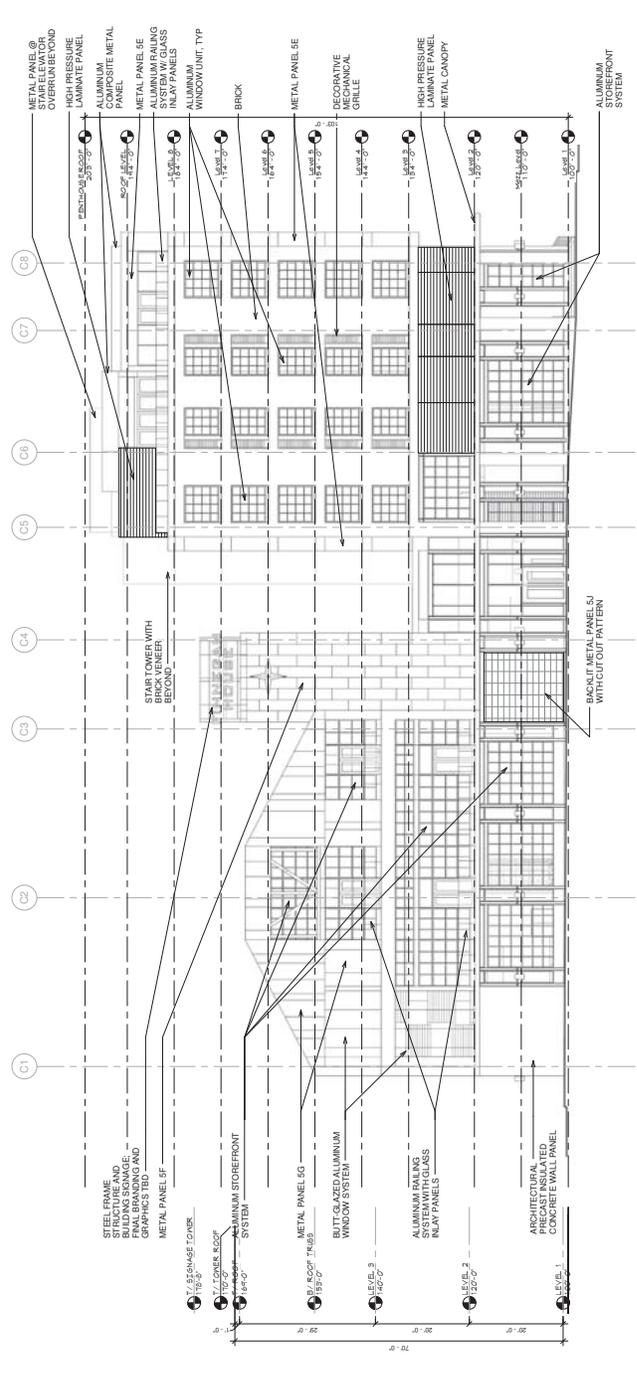
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HOTEL AND BREWERY
ELEVATIONS

A.2.1a.1



1 HOTEL - SOUTH ELEVATION
338'-11 1/2"



2 BREWTEL - WEST ELEVATION
338'-11 1/2"



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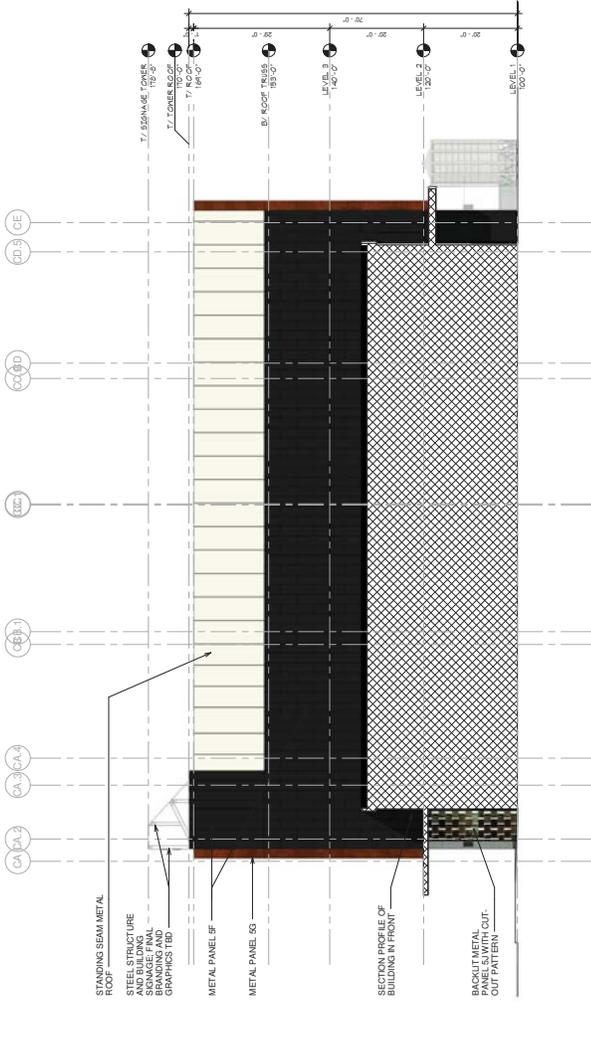
LAND USE APPLICATION
JANUARY 29, 2016

ORIGINAL ISSUE: 01/29/16
REVISED: [Date]

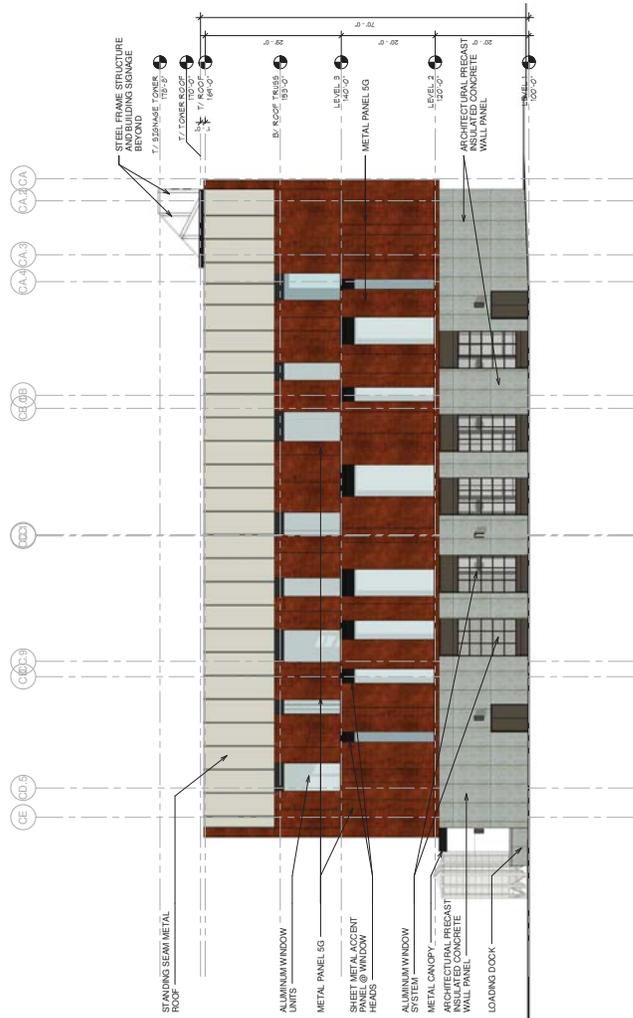
No. Description Date

2.1.15.02
PROJECT NUMBER
ESC
DATE: 01/29/16

KA BLOCK REDEVELOPMENT
HOTEL AND BREWERY ELEVATIONS
A.2.1b



1. BREWERY - SOUTH ELEVATION
1/29 3:32 PM '16



2. BREWERY - NORTH ELEVATION
1/29 3:32 PM '16



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DATE: 01/29/16
PROJECT: KA BLOCK REDEVELOPMENT
DRAWN BY: [Name]
CHECKED BY: [Name]
DATE: 01/29/16

SCALE: 1/8" = 1'-0"

NOT FOR CONSTRUCTION

LAND USE APPLICATION
JANUARY 29, 2016

ORIGINAL ISSUE: 01/29/16

REVISED: [Date]

No. Description Date

2.1.1503 Project Update

Author: [Name]

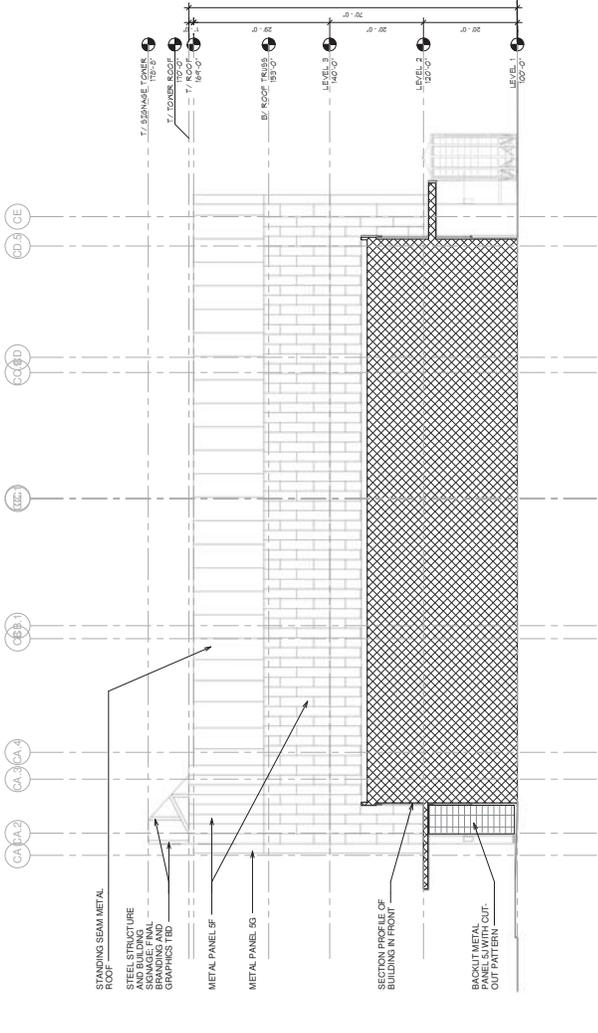
Checker: [Name]

Reviewer: [Name]

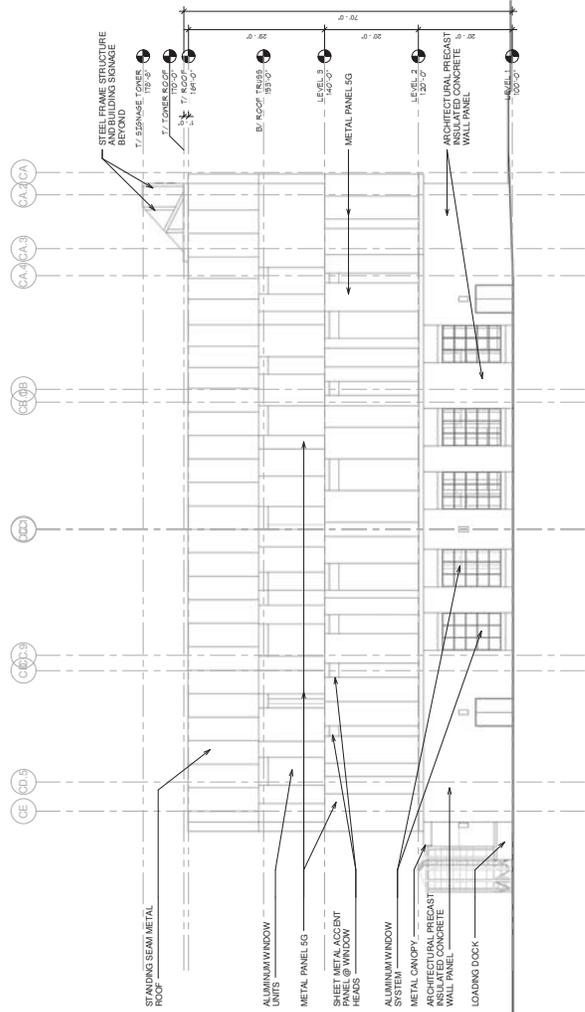
KA BLOCK REDEVELOPMENT

HOTEL AND BREWERY ELEVATIONS

A.2.1b.1



1 BREWERY - SOUTH ELEVATION
1/29/16



2 BREWERY - NORTH ELEVATION
1/29/16



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RETRIEVAL SYSTEM.

DATE: 01/29/16
PROJECT: KA BLOCK REDEVELOPMENT
DRAWN BY: [Name]

NOT FOR CONSTRUCTION

LAND USE APPLICATION
JANUARY 29, 2016

ORIGINAL ISSUE: 01/29/16

REVISIONS:

No. Description Date

2.1.1502

Project Number

Author

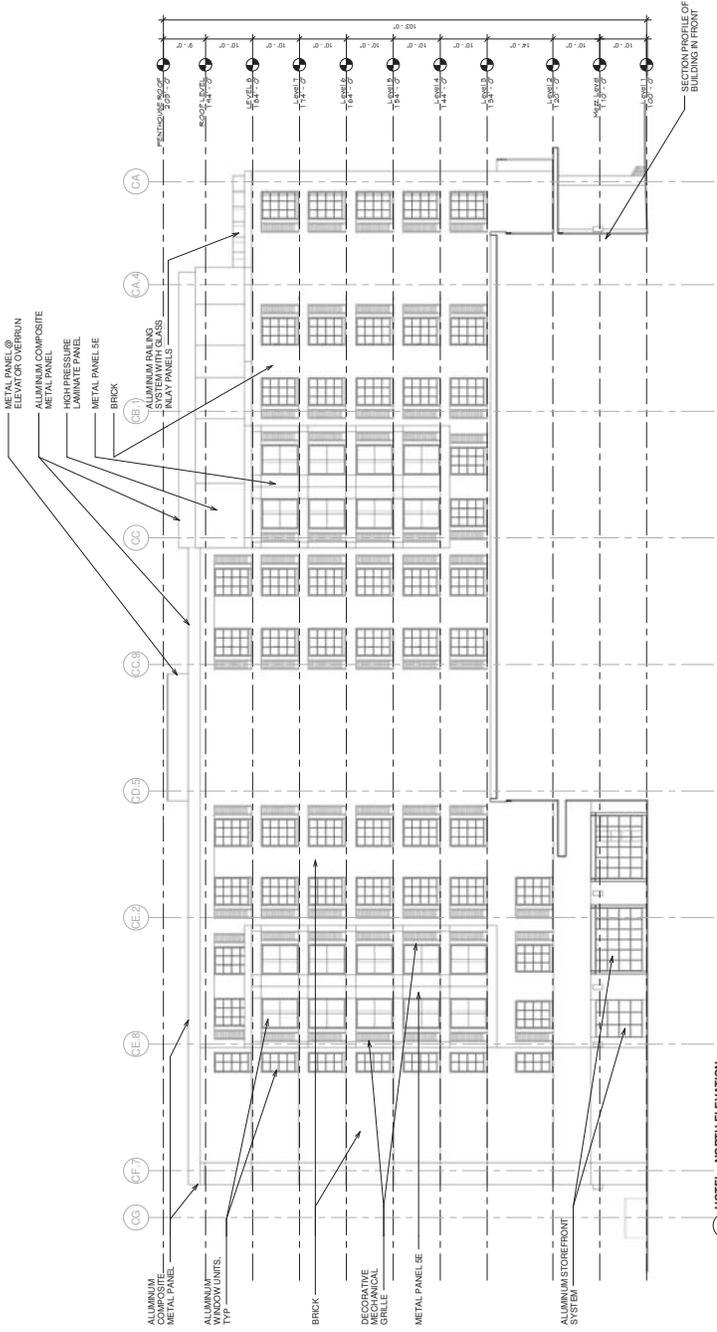
Checker

Reviewer

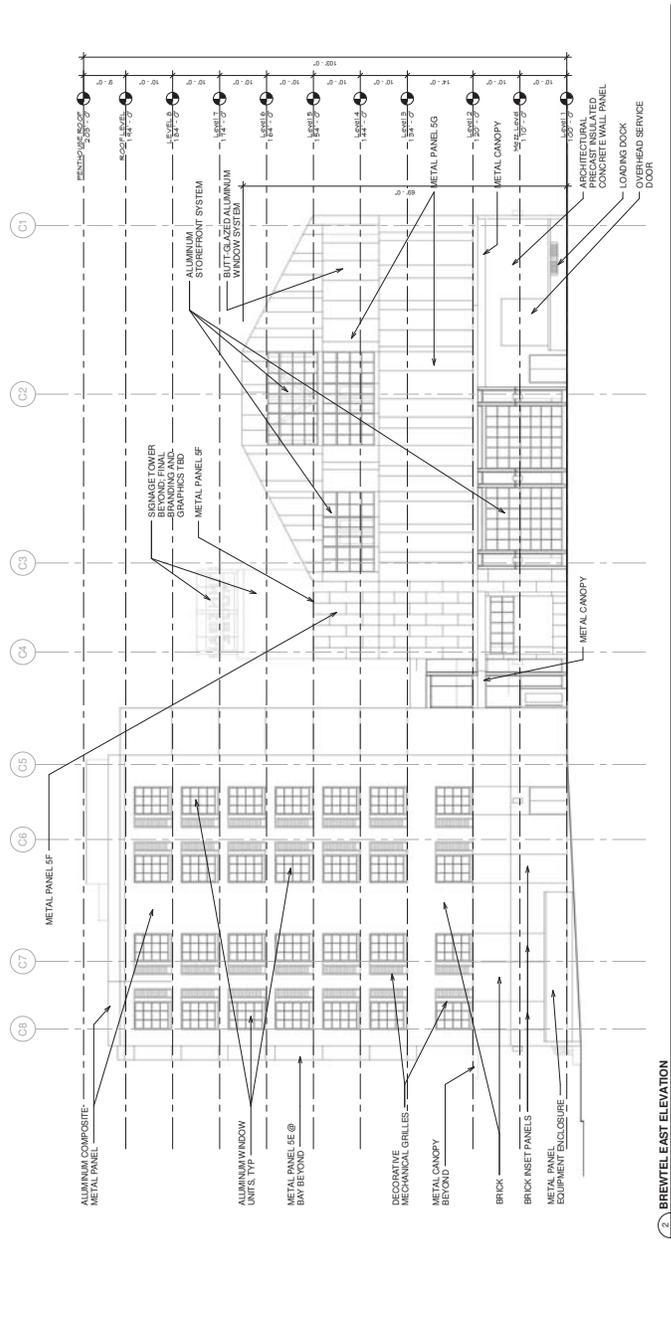
KA BLOCK REDEVELOPMENT

HOTEL AND BREWERY
ELEVATIONS

A.2.1.c.1



2.1 HOTEL - NORTH ELEVATION
202-110



2.2 BREWTELE EAST ELEVATION
202-110



ESC is a licensed professional architect
with offices in Minneapolis, MN
and St. Paul, MN. For more information,
please contact us at 612.339.3332
or visit our website at
www.escarchitect.com

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DATE: 01/29/16
DRAWN BY: [Redacted]
CHECKED BY: [Redacted]

NOT FOR CONSTRUCTION

LAND USE
APPLICATION
JANUARY 29, 2016

ORIGINAL ISSUE: 01/29/16

REVISIONS

No. Description Date

001

2.15.2016

PROJECT NUMBER

ESC

DATE

PROJECT NAME

ADDRESS

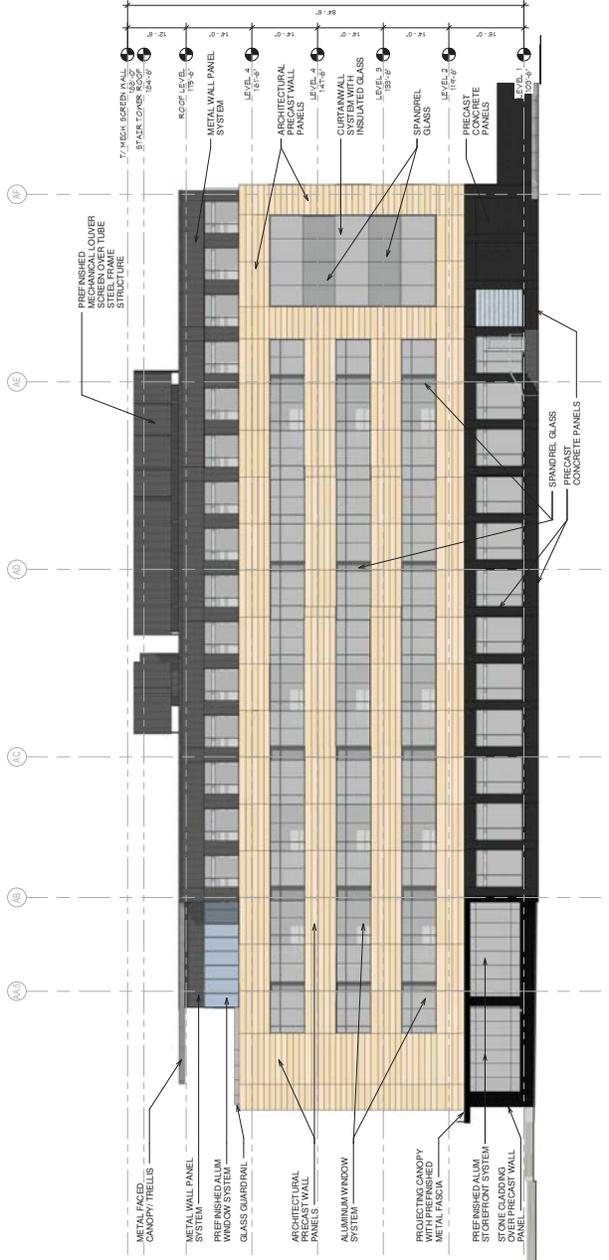
KA BLOCK REDEVELOPMENT

OFFICE ELEVATIONS

A.2.2a



1 KA Office North Elevation
DATE: 01/29/16



2 KA Office South Elevation
DATE: 01/29/16



ERIC S. LUTHELIAN, PRINCIPAL ARCHITECT
ERIC S. LUTHELIAN ARCHITECTS
1000 W. WASHINGTON AVENUE, SUITE 1100
MINNAPOLIS, MINNESOTA 55401
TEL: 612.339.5392
F: 612.339.5392
WWW.ESLARCHITECT.COM

1. THESE NOTES SHALL BE READ IN CONJUNCTION WITH THE ARCHITECTURAL DRAWINGS AND SPECIFICATIONS.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE CITY OF MINNAPOLIS.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE CITY OF MINNAPOLIS.

DATE: _____
PROJECT: _____
DRAWING: _____

NOT FOR CONSTRUCTION

LAND USE
APPLICATION
JANUARY 29, 2016

ORIGINAL ISSUE: 01/29/16

REVISED:
No. Description Date

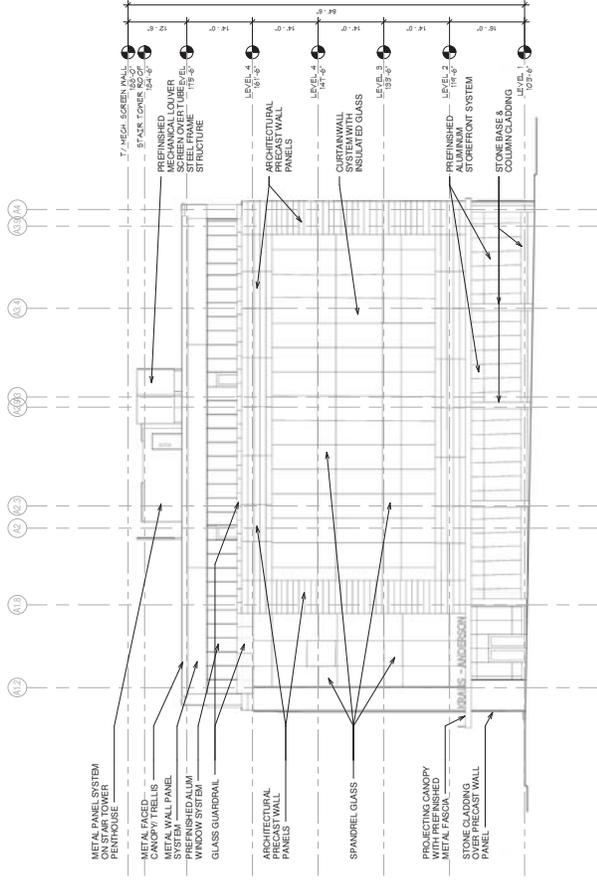
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CHECKER: _____
DATE: _____

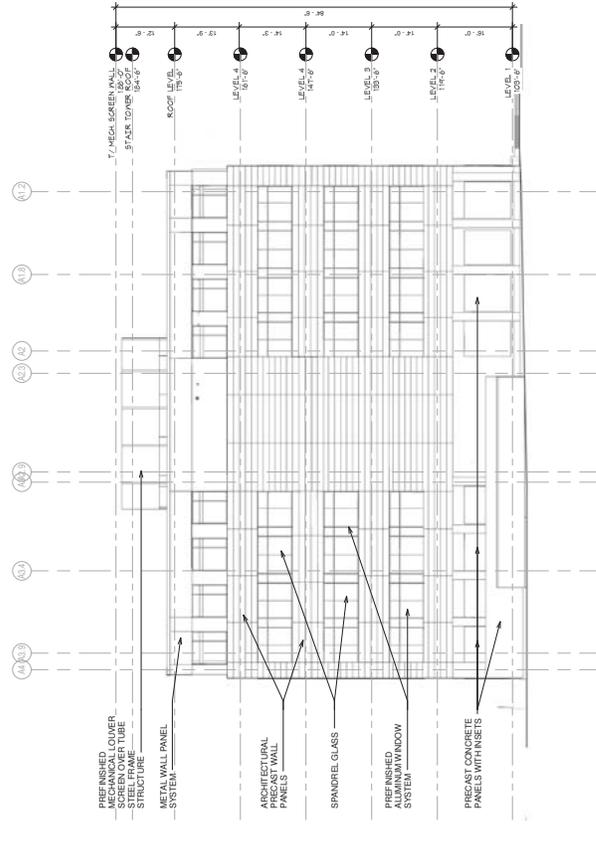
KA BLOCK REDEVELOPMENT

OFFICE ELEVATIONS

A.2.2b.1



KA Office West Elevation



KA Office East Elevation

January 29, 2016

Council Member Lisa Goodman
350 S. 5th Street, Room 307
Minneapolis, MN 55415

Re: Kraus-Anderson Headquarters Block Land Use Application

Dear Council Member Goodman:

On behalf of Kraus-Anderson, Incorporated and Engelsma Limited Partnership (together, "KA") we are writing to let you know we are submitting the Land Use Application and Preliminary Development Review for the redevelopment of the KA block, located at 523 South Eighth Street, Minneapolis, MN 55403. We thank you for your input and suggestions to improve the project.

I. DESCRIPTION OF THE PROJECT

A. In General

The vision of the proposed KA redevelopment is to transform an entire city block, consisting of a surface parking lot and antiquated office building, into a vibrant, high-density mixed use destination in East Downtown. This area of East Downtown is currently characterized by several large surface parking lots and is in need of creative infill redevelopment. The proposed project can also serve as a catalyst for future improvements and investment along the Portland Avenue Corridor.

The proposed redevelopment includes a mix of compatible uses within a composition of high quality individual buildings set within a full block master plan offering several public urban design amenities and benefits:

1. A new Kraus-Anderson corporate home office of approximately 100,000 square feet. KA has been located on this site for over 75 years and wishes to retain and expand its presence in Minneapolis for decades to come and help fulfill the Minneapolis 2025 Vision.
2. A new Type 1 residential midrise building of approximately 306 dwelling units featuring innovative design to attract a full range of millennials to baby boomers who want to be part of this new exciting part of Minneapolis and help fulfill the Minneapolis 2025 Vision.

3. A new 161-key boutique hotel that is part of the Finnegan's vision for a boutique hotel to support the overall Finnegan's integrated urban brewery / innovation concept.
4. A Finnegan's Brewery, Event Center and FINNOvation office space.
 - a. The FINNOvation Lab has three exciting elements:
 - i. A Collaborative Community for Social Entrepreneurs: The FINNOvation Lab will be a community of companies with revenue generating, sustainable business models that create living wage jobs in the community and provide positive social impact.
 - ii. For Businesses That Do Well and Do Good: The FINNOvation Lab will cater specifically to social entrepreneurs and innovators – businesses that do well and do good. It will provide a tailored ecosystem of support services from the start up to scaling phase of their business.
 - iii. Designed To Enhance Collaboration And Cross Pollination Of Ideas: The Lab space will be purposely designed to enhance collaboration and cross pollination of ideas among the diverse individuals, businesses and organizations that become a part of the lab community. It will be a hub for those passionate about social innovation, creativity and collaboration.
 - b. Finnegan House is a destination development, combining the rapidly growing trends of the craft beer movement, collaborative workspaces, social entrepreneurship and experiential overnight accommodations to create a positive impact on the community and greater society. Finnegans will be the first beer company in the world to donate 100 percent of its profits back to the community, and will launch its first ever brewery and experiential brewery lodging experience with 161 guest rooms at Finnegan House. This community building space will also include an outdoor courtyard and event space with planned programming and events designed to engage the neighborhood and the greater community. A restaurant and bar will be located inside Finnegan House, to provide food and beverage services to the entire facility, including catering and room service.

B. Key Overall Design Features and Public Benefits

1. Public Realm and Streetscape:
 - a. Complete rebuilding and enhancements to the surrounding urban sidewalks at Portland Avenue, 9th Street, 5th Avenue and 8th Street.

- b. All four sidewalk areas will feature green boulevards with street trees, new regular and decorative sidewalk pavement, street lighting, landscaping, and outdoor seating areas.
 - c. Additional setbacks to provide a wider, more pedestrian friendly environment.
 - d. Multipurpose service/drop off courtyard: Within the site will be a multi-purpose hardscape courtyard that can be used by the surrounding new development for staging a variety of activities, gatherings and outdoor dining.
 2. Parking Strategy: All parking will be provided below street level in a two-level parking facility with limited access located off of 8th Street South. There will be no surface parking lots. Parking for the hotel and Innovation Center will be provided through an offset, shared parking strategy.
3. Architectural Design:
 - a. Architectural design approach: The overall architectural strategy features three distinctly different building aesthetics within a harmonious overall block massing approach. The visual intent is the creation of a full block redevelopment that has grown organically over time. Type 1, 100-year concrete and steel construction will be used throughout.
 - b. Streetscape public realm shaped by high density/active architecture: The architecture on all sides of the block, shaping the streetscape public realm, will feature a variety of high quality, engaging facades, multiple entry points, and significant expanses of transparent window area to create an active and comfortable public pedestrian environment.
4. Stormwater Management: The stormwater management design will follow City of Minneapolis best practices.
5. Green And Sustainable Features: The design will offer a pedestrian and transit-oriented community that allows residents to live, work and play without the dependence on daily automobile usage. The development team is committed to the sustainable design principles reflected in the City's Comprehensive Plan. Our sustainable design mission is to promote livable communities through the use of energy efficient systems, green building practice, reduced dependency on automobiles, creative density, high quality pedestrian and bicycle public realm and preservation of natural resources. This mixed use project will feature a series of green elements including green construction and demolition practices, green material specifications, thermal high-efficiency windows, and exterior envelope systems, and participation in the Xcel Energy Design Assistance Program.

January 29, 2016

Elliot Park Neighborhood, Inc.
Attn: Kim Forbes, President
609 10th Street South, Suite 170
Minneapolis, MN 55404

Re: Kraus-Anderson Headquarters Block Land Use Application

Dear EPNI Board:

On behalf of Kraus-Anderson, Incorporated and Engelsma Limited Partnership (together, "KA") we are writing to let you know we are submitting the Land Use Application and Preliminary Development Review for the redevelopment of the KA block, located at 523 South Eighth Street, Minneapolis, MN 55403. Thank you for the helpful feedback you have given to the project over the several meetings we have had with EPNI.

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A. In General

The vision of the proposed KA redevelopment is to transform an entire city block, consisting of a surface parking lot and antiquated office building, into a vibrant, high-density mixed use destination in East Downtown. This area of East Downtown is currently characterized by several large surface parking lots and is in need of creative infill redevelopment. The proposed project can also serve as a catalyst for future improvements and investment along the Portland Avenue Corridor.

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II. LAND USE APPLICATIONS

To the best of our knowledge, the applications required for the project are:

- A. Planned Unit Development (“PUD”);
- B. Conditional use permit for height;
- C. Site plan review;
- D. Registered Land Survey; and
- E. Signage (as part of PUD application).

III. ADDRESS OF PROPERTY

523 South Eighth Street, Minneapolis, MN 55404

IV. APPLICANT’S CONTACT INFORMATION

Burton L. Coffin
Elness Swenson Graham Architects Inc.
500 Washington Ave S. #1080
Minneapolis, MN 55415

Thank you. We look forward to continuing to work with you on this exciting project.

Sincerely,

Elness Swenson Graham Architects, Inc.

A handwritten signature in black ink, appearing to read "Burton Coffin". The signature is stylized and cursive.

Burton L. Coffin, Associate AIA
Vice President

Cc: M. Korsh, D. Graham, K. Lamb

January 29, 2016

Downtown Minneapolis Neighborhood Association
Attn: Nicholas Cichowicz, Chair
40 S. 7th Street, Suite 212 PMB 172
Minneapolis, MN 55402

Re: Kraus-Anderson Headquarters Block Land Use Application

Dear DMNA Board:

On behalf of Kraus-Anderson, Incorporated and Engelsma Limited Partnership (together, "KA") we are writing to let you know we are submitting the Land Use Application and Preliminary Development Review for the redevelopment of the KA block, located at 523 South Eighth Street in the Elliot Park Neighborhood, adjacent to the Downtown Minneapolis Neighborhood Association. We are excited to be a part of the significant revitalization that is ongoing in this area.

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- E. Signage (as part of PUD application).

III. ADDRESS OF PROPERTY

523 South Eighth Street, Minneapolis, MN 55404

IV. APPLICANT’S CONTACT INFORMATION

Burton L. Coffin
Elness Swenson Graham Architects Inc.
500 Washington Ave S. #1080
Minneapolis, MN 55415

Sincerely,

Elness Swenson Graham Architects, Inc.

A handwritten signature in black ink, appearing to read "Burton Coffin". The signature is stylized and cursive.

Burton L. Coffin, Associate AIA
Vice President

Cc: M. Korsh, D. Graham, K. Lamb



February 23, 2016

Hilary Dvorak, Principal Planner
Minneapolis Community Planning and Economic Development
250 South Fourth Street, Room 300
Minneapolis, MN 55415

Re: Kraus Anderson development in Elliot Park

Dear Ms. Dvorak:

For the last two years our community has been engaged in discussions with Kraus Anderson regarding the future use and development of their property in the Elliot Park neighborhood. While several of their early development proposals received a tepid community response we were pleased that KA remained steadfast in their commitment to maintain their company headquarters here, building upon their 90 plus-year legacy as Elliot Park neighbors.

We were even more pleased when Kraus Anderson introduced this current proposal. It is a dynamic development that incorporates much of the vision inherent in the Elliot Park Master Plan (2002) and Design Guidelines (2008), incorporating a vibrant mix of uses within four separate eclectic buildings. This was not an easy undertaking, especially as two of the buildings nudged the South Ninth Street Historic District, a valuable asset cherished by the community. Sensitivity to the neighborhood's historic architecture is depicted in the residential row house style entries on Portland Avenue and South Ninth Street, the exterior fenestration that echoes the bay windows on nearby buildings and the selection of complementary materials.

The neighborhood is ever conscious of growing a safer public realm and numerous large transparent windows, generous landscaping and a courtyard for pedestrian and community use all combine for a pedestrian friendly streetscape. This is an impressive full-block development that is packed with interesting and thoughtful components that work together in spirited ways. We anticipate that the completed project will have a vigorous impact on revitalizing this corner of the community.

The neighborhood response has been extremely positive and the following motion was approved at the January 21, 2016 Building, Land Use & Housing (BLUH) meeting by the community: ***Motion to support the Kraus Anderson mixed-use development proposal as presented to the community today [January 21, 2016].*** This motion was subsequently ratified by the EPNI Board of Directors at their regular meeting on February 8, 2016. Please contact me if you have any questions.

Sincerely,

Lynn Regnier, Executive Director

cc. 7th Ward Council Member Lisa Goodman
Burt Coffin, Vice President, ESG Architects
John Campobasso, VP Director of Marketing, Kraus Anderson

609 10th Street South
Suite 170
Minneapolis, MN 55404
612-335-5846
www.elliottpark.org